



City of Plymouth Planning Commission

Regular Meeting Agenda

Wednesday, June 12, 2024 – 7:00 p.m.
City Hall & Online Zoom Webinar

City of Plymouth
201 S. Main
Plymouth, Michigan 48170

www.plymouthmi.gov
Phone 734-453-1234

<https://us02web.zoom.us/j/89981923756>

Passcode: 938090

Webinar ID: 899 8192 3756

1. **CALL TO ORDER**
 - a) Roll Call
2. **CITIZENS COMMENTS**
3. **APPROVAL OF THE MINUTES**
 - a) Approval of the May 8, 2024 meeting minutes
4. **APPROVAL OF THE AGENDA**
5. **COMMISSION COMMENTS**
6. **PUBLIC HEARINGS**
 - a) **SP 24-02:** 413 N. Main; Aqua Fine Dining, Special land use and site plan review
7. **OLD BUSINESS**
 - a) Master Plan Discussion – public engagement survey results
8. **NEW BUSINESS**
 - a) **PUD24-01:** 230 Wilcox, Preliminary PUD
9. **REPORTS AND CORRESPONDENCE**
 - a) Article discussion
10. **ADJOURNMENT**

Citizen Comments - This section of the agenda allows up to 3 minutes to present information or raise issues regarding items not on the agenda. Upon arising to address the Commission, speakers should first identify themselves by clearly stating their name and address. Comments must be limited to the subject of the item.

Meetings of the City of Plymouth are open to all without regard to race, sex, color, age, national origin, religion, height, weight, marital status, disability, or any other trait protected under applicable law. Any individual planning to attend the meeting who has need of special assistance under the Americans with Disabilities Act (ADA) should submit a request to the ADA Coordinator at 734-453-1234 ext. 234 at least two working days in advance of the meeting. The request may also be submitted via mail at 201 S. Main St. Plymouth, MI 48170, or email to clerk@plymouthmi.gov.

GOAL AREA ONE - SUSTAINABLE INFRASTRUCTURE

OBJECTIVES

1. Identify and establish sustainable financial model(s) for major capital projects, Old Village business district, 35th District Court, recreation department, and public safety
2. Incorporate eco-friendly, sustainable practices into city assets, services, and policies; including more environmentally friendly surfaces, reduced impervious surfaces, expanded recycling and composting services, prioritizing native and pollinator-friendly plants, encouraging rain gardens, and growing a mature tree canopy
3. Partner with or become members of additional environmentally aware organizations
4. Increase technology infrastructure into city assets, services, and policies
5. Continue sustainable infrastructure improvement for utilities, facilities, and fleet
6. Address changing vehicular habits, including paid parking system /parking deck replacement plan, electric vehicle (EV) charging stations, and one-way street options

GOAL AREA TWO – STAFF DEVELOPMENT, TRAINING, AND SUCCESSION

OBJECTIVES

1. Create a 5-year staffing projection
2. Review current recruitment strategies and identify additional resources
3. Identify/establish flex scheduling positions and procedures
4. Develop a plan for an internship program
5. Review potential department collaborations
6. Hire an additional recreation professional
7. Review current diversity, equity, and inclusion training opportunities
8. Seek out training opportunities for serving diverse communities

GOAL AREA THREE - COMMUNITY CONNECTIVITY

OBJECTIVES

1. Engage in partnerships with public, private and non-profit entities
2. Increase residential/business education programs for active citizen engagement
3. Robust diversity, equity, and inclusion programs
4. Actively participate with multi-governmental lobbies (Michigan Municipal League, Conference of Western Wayne, etc.)

GOAL AREA FOUR - ATTRACTIVE, LIVABLE COMMUNITY

OBJECTIVES

1. Create vibrant commercial districts by seeking appropriate mixed-use development, marketing transitional properties, and implementing Redevelopment Ready Communities (RRC) practices
2. Improve existing and pursue additional recreational and public green space opportunities and facilities for all ages
3. Develop multi-modal transportation plan which prioritizes pedestrian and biker safety
4. Improve link between Hines Park, Old Village, Downtown Plymouth, Plymouth Township, and other regional destinations
5. Maintain safe, well-lit neighborhoods with diverse housing stock that maximizes resident livability and satisfaction
6. Modernize and update zoning ordinance to reflect community vision
7. Implement Kellogg Park master plan

Planning Commission 2024 Goals

1. Complete the master plan review
2. Engage in a training session
3. Review a compatibility ordinance

“The government in this community is small and accessible to all concerned.”

-Plymouth Mayor Joe Bida November 1977

City of Plymouth
Planning Commission Public Hearing Notice
201 S. Main Street Plymouth, Michigan 48170
Website: www.plymouthmi.gov Phone: (734) 453-1234 ext. 232

A regular meeting of the Planning Commission will be held on Wednesday, June 12, 2024, at 7:00 P.M. located at City Hall and online via Zoom to consider the following:

SP 24-02: 413 N. Main, Special land use and site plan review for expansion of outdoor dining area

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Publish: Tuesday, May 28, 2024



Plymouth Planning Commission
Regular Meeting Minutes
Wednesday, May 8, 2024 - 7:00 p.m.
Plymouth City Hall 201 S. Main

City of Plymouth
Plymouth, Michigan 48170-1637

www.plymouthmi.gov
734-453-1234

1. CALL TO ORDER

Chair Karen Sisolak called the meeting to order at 7:00 p.m.

Present: Chair Sisolak, Vice Chair Scott Silvers, Commissioners Sidney Filippis, Zach Funk, Joe Hawthorne, Trish Horstman, Kyle Medaugh, Hollie Saraswat, and Eric Stalter

Also present: Planning and Community Development Director Greta Bolhuis, Planning Consultant Sally Elmiger

2. CITIZENS COMMENTS

Laura Lycette, 480 Parkview, voiced her concerns about a short-term rental property in her neighborhood.

3. APPROVAL OF MEETING MINUTES

Filippis offered a motion, seconded by Medaugh, to approve the minutes of the April 10, 2024 meeting.

There was a voice vote.

Hawthorne and Stalter abstained.

MOTION PASSED 7-0

4. APPROVAL OF THE AGENDA

Saraswat offered a motion, seconded by Silvers, to amend the agenda by correctly identifying SP24-05 as a public hearing, which would also move it above old business.

There was a voice vote.

MOTION PASSED UNANIMOUSLY

5. COMMISSION COMMENTS

There were no commission comments.

6. PUBLIC HEARINGS

- a. SP 24-05: 650 Church, Special land use and site plan review for pickleball courts

PARC representatives Mark Malcolm and Ted Barker provided an overview of their project and responded to questions in the Carlisle Wortman report about hours, providing a bike rack, parking, and noise. Sisolak opened the public hearing at 7:15 p.m.

The following residents spoke in support of the project:

Eric Nemes, 140 S. Union

Debra Kuptz, 997 Carol

Ron Picard, 1373 Sheridan

Karen Jallo, 505 McKinley
Ellen Elliott, 404 Irvin

Sisolak closed the public hearing at 7:18 p.m.

Elmiger said the plan should include protection for the large tree adjacent to the court area.

Motion

Silvers offered a motion, seconded by Stalter, to approve SP 24-05 for special land use.

Findings of Fact

The project meets the objectives of the master plan.

The project includes recommendations in the Carlisle Wortman report.

Conditions

The applicant is to provide a bike rack.

No additional landscaping is required.

The applicant is to provide protection for the adjacent mature tree abutting the pickleball court location during construction.

The applicant is to address any noise or sound complaint with the Community Development Director to reach resolution.

No additional parking is required.

There was a voice vote.

MOTION PASSED UNANIMOUSLY

Motion

Silvers offered a motion, seconded by Funk, to approve SP 24-05 for site plan review.

Findings of Fact

The project meets the objectives of the master plan.

The Planning Commission has reviewed the Carlisle Wortman report regarding the site plan.

Condition

The applicant is to work with the Building Official to approve a sign permit.

There was a voice vote.

MOTION PASSED UNANIMOUSLY

7. OLD BUSINESS

a. PUD 23-01: 1100 W. Ann Arbor Trail, Revised Preliminary PUD

Champion Development Representative Doug Hamborsky, First Church of Christ, Scientist Representative Leslie Evans, and Attorney John Gaber reviewed the changes made to the plan since the last time they came before the Planning Commission, including the following:

- Reducing the density from a total of 24 residential units to a new total of 20 units (17 Townhouses + 3 stand-alone units)
- Adding public benefits
- Providing additional parking spaces that may be shared with Tonquish Creek Manor

- Providing two guest spaces in front of every 3 bedroom Townhouse and 1 guest space in front of the two 2 bedroom townhouses
- Commissioning the creation of a creek restoration environmental / ecological master plan report and the creation of a traffic analysis report to study traffic conditions for the project design.

The public benefits they cited include:

1. Provide shared parking for the adjacent Tonquish Manor operations
2. Provide Tonquish Creek and trail restoration plan
3. Provide full restoration of the Brookside section of the creek bank and partial trail clean up
4. Provide select Tonquish Creek trail improvements
5. Deed a portion of property along the bank of the creek to the City
6. Construct a new access stair at the end of the creek
7. Provide a new pocket park at the Reading Room bookstore
8. Provide a new pocket park along the creek
9. Provide significant new tax revenue for the city
10. Produce significant net traffic reduction
11. Addition of 20 new families for the downtown community
12. Site to be updated with all new construction and stormwater utilities
13. Provide significant additional guest parking for townhouse units

The following residents spoke in opposition to the project.

Tom Doran, 1191 Sheridan
 David Pierce, 1147 W. Ann Arbor Trail
 Denise Varner, 1428 Sheridan
 Julie Nelson, 1464 W. Ann Arbor Trail
 Mike Mountain, 1211 Sheridan
 Elizabeth Volaric, 216 N. Holbrook
 Karen Jallo, 505 McKinley
 Ben Covington, 360 S. Harvey
 Ron Picard, 1373 Sheridan
 Elaine Attridge, Joel at Ann Arbor Trail
 Fred Brunk, 523 McKinley
 Ellen Elliott, 404 Irvin
 David Sibbold, 1351 Woodland Place
 Scott Lorenz, 1310 Maple
 Alan Ardanowski, 1120 Maple

The following spoke in support of the project.

Mark Wira, Plymouth business owner
 Bill Skubik, Real Estate agent
 Betty Schovers, potential city resident

There was a break from 9:10-9:15 p.m.

Elmiger explained to the group that they were to evaluate the project using nine criteria outlined in the ordinance.

Commission members discussed the project. Items of discussion included the following:

- The reduced density was appropriate

- The public benefit was inadequate – the pocket parks seemed more like landscaping, Tonquish Creek plan didn't include specific work to be performed
- Some were concerned with additional traffic and related safety, one pointed out that traffic was not an issue at the multi-family units at Union and Hamilton, a larger complex
- Greenspace and other impervious surfaces were inadequate
- Concerns about the length of the wall along the West property line
- The placement was awkward in a transitional property, particularly when considering the potential future form-based codes
- The traffic study was actually a trip generation study
- The city did not need property deeded to it because there is already an easement
- The stairs were too steep and would have to be maintained by the city
- The property was undersized for a church, per existing ordinance requirements

Elmiger said the Commission's recommendation needed to be based on the facts of this project.

The developer asked for clarification on how much greenspace would be adequate, and it was noted that this was not specific in the ordinance.

The nine criteria for a PUD were reviewed.

The developer responded to the comments that the public benefit was inadequate, stating that they have put a significant amount of work into a benefit package they feel is substantial and it was unfair to give the efforts zero value. He explained that the mission of the project was to keep the church on the property.

Elmiger suggested having the administration draft a motion based on the facts of the project and the criteria of the ordinance and present it to the Planning Commission at their next meeting. This suggestion was rejected.

The requirement that a public hearing be held for a preliminary PUD was discussed. Economic Development Director John Buzuvis explained past practices in scheduling a public hearing for a PUD.

8. NEW BUSINESS

There was no new business

9. REPORTS AND CORRESPONDENCE

City Commission Liaison Brock Minton thanked Planning Commission members for their thoughtful discussion.

10. ADJOURNMENT

Saraswat offered a motion, seconded by Funk, to adjourn the meeting at 10:58 p.m.

There was a voice vote.

MOTION PASSED UNANIMOUSLY

RECEIVED

MAY 21 2024

CITY OF PLYMOUTH
 COMMUNITY DEVELOPMENT

CITY OF PLYMOUTH
 SITE PLAN REVIEW APPLICATION

Community Development Department
 201 S. Main Street Plymouth, MI 48170
 Ph. 734-453-1234 ext. 232
www.plymouthmi.gov

ITEM 8) a

I. Site/Project Information

Site Address 413 main street	Current Zoning Classification b3	Date of Application 3.18.24
Name of Property Owner Victor Nikollbibaj	Phone Number	
Mailing Address 413 main street	Email Address (Required) vitor413@yahoo.com	
City plymouth	State mi	Zip Code 48170

II. Applicant and Contact Information

Indicate Who the Applicant Is. If Property Owner, Skip to Section III.	Architect <input checked="" type="checkbox"/>	Developer <input type="checkbox"/>	Engineer <input type="checkbox"/>	Lessee <input type="checkbox"/>
Applicant/Company Name composition workshop	Phone Number 313-478-1547			
Applicant/Company Address 800 junction st	City plymouth	State mi	Zip Code 48170	
Email Address (Required) jkorf@cmpworkshop.com				

III. Site Plan Designer and Contact Information

Site Plan Designer Company Name composition workshop	Phone Number 313-478-1547			
Company Address 800 junction st	City plymouth	State mi	Zip Code 48170	
Registration Number 1301057954	Expiration Date 3/14/2026	Email Address (Required) jkorf@cmpworkshop.com		

IV. Type of Project (Please Select All that Apply)


V. Historic District

<input checked="" type="checkbox"/> Commercial	<input type="checkbox"/> Multi-Family	<input type="checkbox"/> New	<input type="checkbox"/> Remodel	<input type="checkbox"/> Change of Use	Is this project located in the Historic District? <input type="checkbox"/> Yes <input type="checkbox"/> No
<input type="checkbox"/> Mixed Use	<input type="checkbox"/> Industrial	<input type="checkbox"/> Addition	<input type="checkbox"/> Interior Finish	<input checked="" type="checkbox"/> Special Land Use	

VI. Description of Work

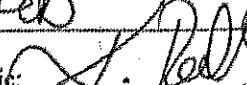
addition of a 360 sq/ft outdoor eating area near the northwest corner of the building, utilizing 2 parking spaces.

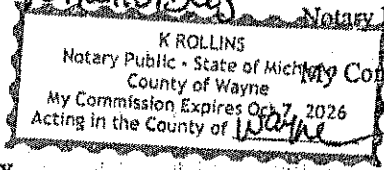
VII. Applicant Signature

Signature of Applicant 	Date 2.18.24
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VIII. Property Owner Signature

Signature of Property Owner 	Date 02.20.24
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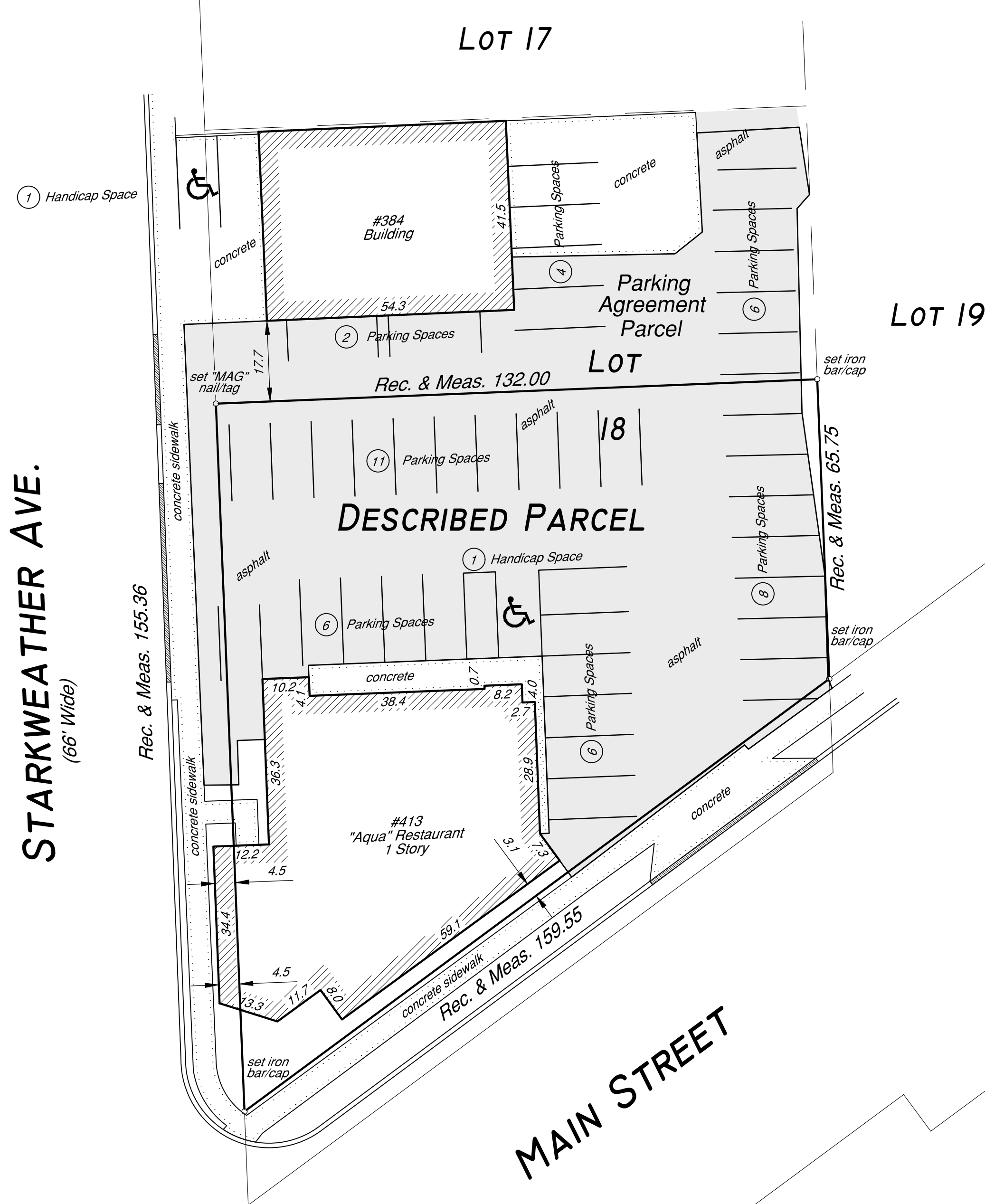
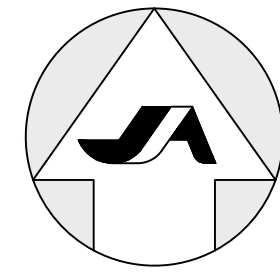
Subscribed and sworn before me this 20 day of Feb, 2024
Vitor Nikalbibas Notary Public: 



My Commission expires: 10/07/2026

For Office Use Only

		YES/DATE	NO	N/A
1.	Pre-Application Meeting			
2.	Digital Copy of Application Package			
3.	Public Hearing Notice			
4.	CWA Review			
5.	Municipal Services Review			
6.	Fire Department Review			
7.	Engineering Review			



STARKWEATHER AVE.
(66' Wide)

MAIN STREET

Parking Spaces
43 Standard
2 Handicap

LEGAL DESCRIPTION

LOT 18, EXCEPT THE NORTH 60.00 FEET THEROF, AND EXCEPT THE SOUTHERLY 17.00 FEET THEREOF, OF "ASSESSOR'S PLYMOUTH SUBDIVISION No.2", PART OF THE NORTHWEST 1/4 OF SECTION 26, TOWN 1 SOUTH, RANGE 8 EAST, PLYMOUTH VILLAGE (NOW CITY OF PLYMOUTH), WAYNE COUNTY, MICHIGAN, AS RECORDED IN LIBER 63 OF PLATS ON PAGE 77, WAYNE COUNTY RECORDS. PARCEL CONTAINS 14,593 SQUARE FEET. SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD.



SITE SURVEY

Prepared For: Aqua Fine Dining
Victor 734 353-0603

Jekabson & Associates, P.C.
Professional Land Surveyors
1320 Goldsmith, Plymouth, MI 48170
(734) 414-7200 (734) 455-8000

by: *I. John Jekabson*

Date	13 May 2024
Job no.	77-01-077
Scale	1" = 20'
Drawn	AAH
Checked	JGE
Sheet	1 OF 1



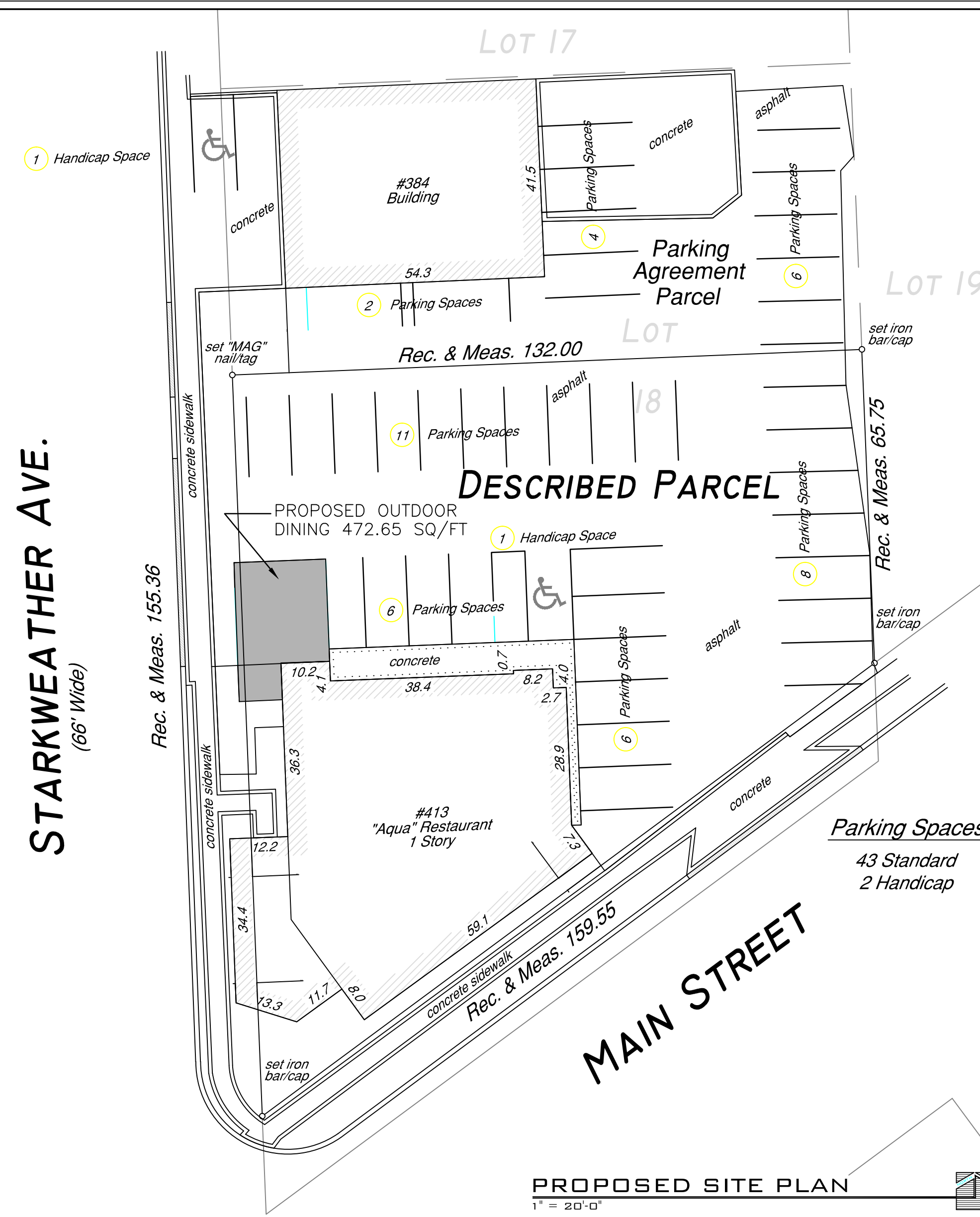
Know what's below.
Call before you dig.



* PICTURES SHOWN ARE OF A PREVIOUS INSTALLATION AND ARE SHOWN TO ILLUSTRATE CONSTRUCTION TYPE AND QUALITY. FOR ACTUAL SIZE SEE PLAN



STARKWEATHER AVE.
(66' Wide)



AQUA

413 N. MAIN STREET
PLYMOUTH, MICHIGAN

SITE DATA

ZONING REQUIREMENTS	B3 GENERAL BUSINESS	
PROPOSED USE:	RESTAURANT	
SETBACKS:	REQUIRED	PROVIDED
FRONT	0' FEET	3'-0" FEET
SIDE YARD	0' FEET	8' FEET WEST 68' FEET EAST
REAR YARD	10' FEET	128'-8.5" FEET
MAXIMUM BUILDING HEIGHT	30 FT 2 STORIES	20' FT 1 STORIES
MIN LOT AREA	2 STORIES	
MAXIMUM LOT COVERAGE		
TOTAL SITE:	EXISTING BUILDING	4004 SQ/FT
	NEW OUTDOOR DINING	472.65 SQ/FT
	DRIVEWAYS + PATIOS	3059.65 SQ/FT

DRAWING INDEX

T1.1	TITLE SHEET / SITE PLAN	ISSUED FOR SPECIAL USE PERMIT 21.FEB.24	PREPARED BY: REFERENCE ONLY
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PROJECT DESCRIPTION

THE PROJECT CONSISTS OF THE ESTABLISHMENT OF A NEW OUTDOOR EATING AREA (480 SQ/FT) COMPRISED OF DECORATIVE WOODEN FENCING (MOVABLE AND REMOVABLE) TO BE INSTALLED AND REMOVED SEASONALLY.

PARKING REQUIREMENTS

AQUA RESTAURANT AND THE BUSINESS IMMEDIATELY TO THE NORTH SHARE A PARKING LOT BY LEGAL AGREEMENT. THE BUSINESS TO THE NORTH SHALL NOT, BY AGREEMENT, PARK ANY CARS IN THE SHARED LOT PAST 5PM. AQUA RESTAURANT OPENS ONLY FOR DINNER.

BY DIVISION XXII, SECTION 78-271 OF THE ZONING ORDINANCE, CITY OF PLYMOUTH:
Establishment for sale and consumption on the premises of beverages, food and refreshments Outside of the downtown development district:
One for each 75 square feet of usable floor area or one for each three persons allowed within the maximum occupancy load as established by local, county or state fire, building or health codes, whichever is greater.

LISTED OCCUPANCY BY FIRE MARSHALL: 109
SPACES REQUIRED BY OCCUPANCY: 109 / 3 = 36.3

BUILDING SQ/FT: 4004 SQ/FT
GROSS FLOOR AREA: 3839 SQ/FT

REQUIRED PARKING: 3839 / 75 SQFT = 51.17

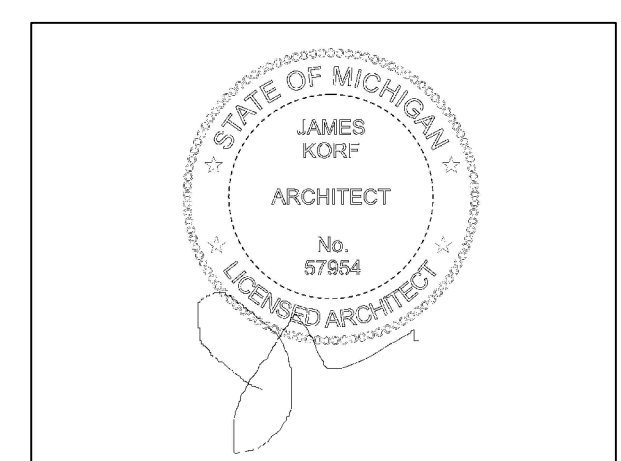
SPACES REQUIRED: 51 SPACES
SPACES PROVIDED: 45 SPACES

OWNER

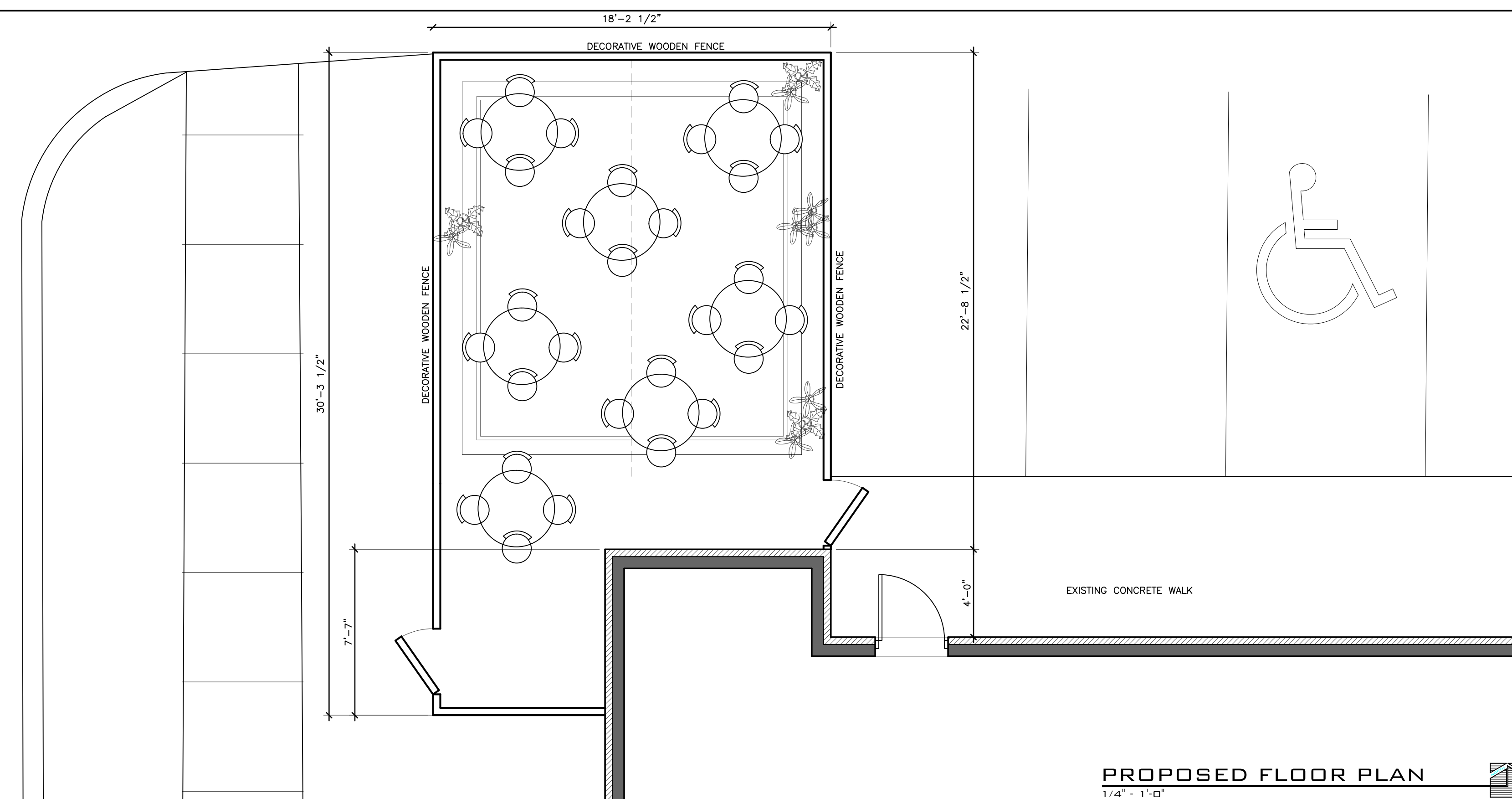
VITOR NIKOLLBIBAJ
413 N. MAIN
PLYMOUTH, MI 48170

ARCHITECT

COMPOSITION WORKSHOP INC
800 JUNCTION ST
PLYMOUTH, MI 48170
313-478-1547
734-425-5226 FAX



THE CONTRACTOR IS REQUIRED TO FIELD VERIFY ALL EXISTING CONDITIONS WITH REGARD TO THE VIABILITY OF EXISTING FOUNDATIONS, STRUCTURAL BEARING LOCATIONS, AND ALL DIMENSIONS, PRIOR TO THE BEGINNING OF CONSTRUCTION.



COMPOSITION WORKSHOP
ARCHITECTURE + INTERIOR DESIGN
800 Junction St.
Plymouth, Michigan, 48170
Phone: 313.478.1547
Fax: 734.425.5226

PROJECT
AQUA
OUTDOOR DINING

DATE	REVISIONS	APP'D
21.FEB.24	ISSUED FOR SPECIAL USE PERMIT	
17.MAY.24	REVISED/RESUBMITTED FOR SPECIAL USE PERMIT	

FILE NUMBER 08_01	DRAWN BY JFK	DATE ISSUED 21NOV23
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SHEET TITLE
TITLE SHEET
SITE PLAN

SHEET NUMBER
ST1.1



Carlisle | Wortman
ASSOCIATES, INC.

117 NORTH FIRST STREET SUITE 70 ANN ARBOR, MI 48104 734.662.2200 734.662.1935 FAX

Date: March 6, 2024
Rev.: May 29, 2024

**Special Land Use and
Site Plan Review
For
Plymouth, Michigan**

Applicant:	Victor Nikollbibaj 413 Main St. Plymouth, MI 48170
Project Name:	Aqua Restaurant Outdoor Seating
Plan Date:	November 21, 2023
Latest Revision:	May 17, 2024
Location:	413 N. Main St.
Zoning:	B-3 – General District
Action Requested:	Special Land Use and Site Plan Approval
Required Information:	Any deficiencies are noted in the report.

PROJECT AND SITE DESCRIPTION

The applicant is proposing to use two (2) adjoining parking spaces to locate a 473-square-foot outdoor dining area on the subject site. The outdoor area is directly adjacent to the applicant's place of business (Aqua Restaurant), and will be blocked off using moveable black wooden fencing. A small portion of the outdoor dining area will be located in the Starkweather Ave. right-of-way.

This change represents an expansion of a use that serves alcohol, and is therefore a Special Land Use in the B-3 District, and requires a Special Use Permit.

An aerial and street view of the subject site is shown in **Figures 1 and 2** below.

Figure 1. Subject Site



Source: Near Maps (Capture 4-7-2023)

Figure 2. Subject Site – Street View



Source: Google Maps (Capture 7-2019)

SPECIAL LAND USE STATUS

The applicant is proposing to expand alcohol service to this outdoor patio, which is a Special Land Use and must meet the Special Land Use standards in Section 78-281.

In addition, the B-3 General Business District (in Section 78-112) states that the Community Development Director shall request a report from the city's Director of Public Safety regarding the possible impacts of the establishment serving alcoholic beverages, and that the Planning Commission will consider this report.

The special land use standards in Section 78-281 are as follows. Our comments regarding each are provided below:

(1) Will be harmonious and in accordance with the general objectives or any specific objectives of the City of Plymouth Master Plan.

CWA Comment:

The 2018 Master Plan:

- Identifies this site as "Mixed Use High Density," which includes a mixture of retail, service, office, recreation and residential uses. It also states that parking shall be located at the rear of the building, or integrated and hidden. (Pg. 12)
- The North Main Street Sub Area Plan also states that parking should be located at the rear of buildings, and that landscape strips or decorative knee walls should separate front yard parking and sidewalk areas. (Pg. 23)
- Lastly, *Figure 12: Primary & Secondary Transportation Routes* map (Pg. 37) indicates that the existing public sidewalk on the east side of Starkweather (adjacent to this site) is too narrow. This public sidewalk also directly abuts the curb, and has no green strip between the curb and sidewalk, placing pedestrians right next to the street. The existing parking lot on the subject site occupies about 5-feet of this right-of-way, directly next to the sidewalk.

Proposed Use

Unlike the Downtown Sub Area Plan, the North Main Street Sub Area Plan doesn't encourage outdoor cafes. However, this Sub Area does call for a mix of retail, service, office, recreation and residential uses. Outdoor eating areas help to establish a "mixed-use" atmosphere.

Proposed Layout

While the use may be consistent (in our opinion) with the Master Plan, the layout of this site is not compliant. The existing parking lot occupies about 5-feet of the right-of-way, is located directly next to the public sidewalk along Starkweather, and there is no landscape separation or knee wall between the parking lot and the street, making it clearly visible to passersby. This condition will not be improved by the proposed outdoor dining layout.

Also, the survey provided shows that this business has constructed a 3- or possibly 4-season enclosed dining room in the Starkweather St. right-of-way. The applicant should confirm that this structure was approved by the City Commission for this location in the right-of-way. If not, then permission needs to be obtained. We recommend conditioning any approval for the new patio on the applicant obtaining this permission.

The outdoor dining area is proposed to be located in the two most westerly parking spaces on the north side of the building. The patio will be in the Starkweather right-of-way by one (1) foot. The proposed dining use should not be placed in the right-of-way. Therefore, the plans should be revised to remove the patio from the right-of-way. We recommend that this be a condition of approval, if granted.

To make the proposal more consistent with the goals of the Master Plan, the Planning Commission may want to discuss the addition of landscape planters along the west side of the dining enclosure fence.

- (2) Will be designed, constructed, operated, and maintained so as to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity and will not change the essential character of the area.**

CWA Comment:

No permanent structures are proposed. Sheet ST1.1 shows photographs of when an outdoor dining area was installed up next to the sidewalk in previous years, with a black fence. In our previous review, we asked if the same fence/umbrellas/chairs shown in the photos be used if the outdoor dining area is approved. A note next to the photos states: "Pictures shown are of a previous installation and are shown to illustration construction type and quality." The plans also show that the outdoor dining area will accommodate 7 tables/28 patrons.

- (3) Will not be hazardous or disturbing to existing or future nearby uses.**

CWA Comment:

The outdoor dining area will be located directly across the street from a single-family residential use. The residential site at 353 Starkweather was approved for rezoning to R-1, Single-Family Residential District, by the City Commission on March 18, 2024. Therefore, an outdoor use that extends into the evening 7-days a week could be disturbing to these neighbors.

The Aqua Restaurant's website indicates that the business is open 4pm – 10pm, 7-days a week. We have the following questions:

- Will the current hours of operation apply to the outdoor dining area?
- Will the outdoor area be used only for patrons eating a meal, or will patrons only using the bar also be able to sit outside?
- Is the applicant proposing any outdoor speakers or music?
- How will the outdoor area be managed to minimize disturbance to nearby residents?

- (4) Will be compatible with adjacent uses of land and will promote the use of land in a socially and economically desirable manner.**

CWA Comment:

As mentioned above, the building across Starkweather to the west of the outdoor was approved for single-family residential use. The building to the north is approximately 2,220 s.f. and contains

two commercial units. One of these units is occupied by an “event” business (accommodating up to 50 people at a time).

The outdoor dining area is compatible with the commercial building to the north, but could be disturbing to residents directly across Starkweather. See our questions above.

- (5) Will be served adequately by essential public services and facilities or that the persons responsible for the establishment of the proposed use will provide adequately any such service or facility.**

CWA Comment:

Essential public services currently serve the Aqua Restaurant. The outdoor dining area does not require any additional public services.

- (6) Will not create excessive additional public costs and will not significantly decrease property values of surrounding properties.**

CWA Comment:

We don't believe the proposed outdoor dining area will significantly decrease property values of surrounding properties.

- (7) Will meet all the requirements and standards of this chapter and any other applicable laws, standards, ordinances, and or/regulations.**

CWA Comment:

See our comments in the remainder of this review for compliance with ordinance requirements.

In summary, we consider *the proposed use* to meet the criteria for the standards in 78-281 and 78-102(2). However, we don't consider the proposed layout to meet the criteria. The applicant needs to provide additional information to answer the questions in this review, and any additional questions Commissioners may have. Also, the Planning Commission may want to discuss modifications to the layout with the applicant to better meet the goals of the Master Plan.

Items to be Addressed: 1) Planning Commission to consider Director of Public Safety report. 2) Applicant to confirm and/or obtain permission from City Commission for 4-season dining structure in the Starkweather right-of-way. Recommend conditioning any approval for the new patio on the applicant confirming/obtaining this permission. 3) Revise plans to remove the patio from the Starkweather right-of-way. Recommend that this be a condition of approval. 4) Planning Commission to discuss the addition of landscape planters along the west side of the dining enclosure fence. 5) Will the current hours of operation apply to the outdoor dining area? 6) Will the outdoor area be used only for patrons eating a meal, or will patrons only using the bar also be able to sit outside? 7) Is the applicant proposing any outdoor speakers or music? 8) How will the outdoor area be managed to minimize disturbance to nearby residents?

SITE PLAN REQUIRED INFORMATION

Per Section 78-247, the submission shall show specific site information. A site layout, including the building to the north, is provided on Sheet ST1.1. All required information has been provided.

Items to be Addressed: None.

PARKING

Parking in the B-3 zoning district requires one space per 75 square feet of usable floor area, or one space for each three persons allowed within the maximum occupancy load.

The plans show 3,839 gross s.f. of floor area for the existing building, and 473 s.f. for the outdoor dining area. We have the following comments/questions:

- 1) No building floor plans are provided, so we are unable to confirm the floor area calculation. The plans calculate required parking using “gross floor area;” however, the ordinance requirements use “usable floor area,” which is defined as follows:

Floor area, usable, for the purposes of computing parking, means that area used for or intended to be used for the sale of merchandise or services, or for use to serve patrons, clients or customers. Such floor area which is used or intended to be used principally for the storage or processing of merchandise, hallways or for utilities or sanitary facilities, shall be excluded from the computation of usable floor area. Measurement of usable floor area shall be the sum of the horizontal areas of the several floors of the building, measured from the interior faces of the exterior walls.

The applicant should provide the “usable floor area” of the existing building, per ordinance definition, to calculate required parking.

- 2) The 4-season dining area should be included in the “usable floor area” figure.
- 3) The applicant should also confirm that the “maximum occupancy” figure (109 persons) includes the 4-season dining area in the right-of-way. If not, it should be added.
- 4) The plans state that this parking area is shared with the multi-unit building to the north, and that an agreement exists that limits the neighbor’s use of the parking lot to times before 5pm. This agreement should be provided to the City to confirm how the lot is being shared between buildings.

As requested, an accurate survey of the property has been provided, showing the existing parking lot configuration and number of spaces. Per the ordinance, the table on the following page shows the required and provided number of parking spaces for this use:

	Required Number of Spaces	Provided Number of Spaces
One space per 75 s.f. usable floor area Or One space for each 3 persons maximum occupancy, Whichever is greater	$3,839 \text{ s.f.} / 75 = 52 \text{ spaces}$ Or $109 / 3 = 37 \text{ spaces}$ 52 spaces*	41 spaces (See below)

*For fractional spaces, the ordinance states that any fraction shall be counted as one (1) space.

Proposed Parking

- a. One (1) parking space in the 11-space bay is in the right-of-way. This condition will need to be approved by the City Commission, if not already approved.
- b. The width of one (1) parking space up against the building is too narrow. The proposed outdoor patio needs to be narrowed by one (1) foot to vacate the last parking space.
- c. The width of the barrier-free aisle is one (1) foot too narrow. The aisle needs to be a minimum of 8-foot wide, and the plans should be revised to show this change. This can be accomplished by narrowing the barrier-free space to 8-feet. (When only one barrier-free space is required in a parking lot, it must meet the “van-accessible” dimensions, which are 8-feet wide for the barrier-free space, with an 8-foot wide aisle.)
- d. It is the City’s policy to not require additional parking for outdoor dining because of the seasonality of the use. Therefore, no additional spaces are required for the outdoor dining area.
- e. The existing number of parking spaces is non-compliant with the required number of spaces. However, since the City doesn’t require additional parking for outdoor dining, we would consider this to be an existing non-conformity that may remain.

Items to be Addressed: 1) Provide “usable” floor area, per the Zoning Ordinance definition of “usable floor area.” 2) Include 4-season dining area in the “usable” floor area figure. 3) Applicant to confirm that maximum occupancy figure includes the 4-season dining area. 4) Applicant to provide shared parking agreement to confirm how parking lot is shared between buildings. 5) Obtain permission from the City Commission to locate one parking space in the Starkweather right-of-way. 6) Narrow proposed outdoor dining so that the width of the adjoining parking space is at least 9-feet wide. 7) Modify plans to indicate 8-foot-wide barrier-free parking space, and 8-foot wide aisle.

RECOMMENDATIONS

The 4-season structure and one parking space currently occupy the Starkweather right-of-way. Confirmation that this structure and parking space location were approved to be located in the right-of-way needs to be provided. If not already approved, then permission to locate this structure and parking space in the right-of-way needs to be obtained from the City Commission.

Regarding the Special Land Use approval of the proposed outdoor seating, in our opinion, the outdoor seating area use meets the vision in the Master Plan for this street, but the design may not be consistent with the Master Plan. While we consider the existing parking an “existing non-conformity,” it’s our opinion that the Planning Commission should have accurate information to determine that the existing parking is sufficient, particularly since more people will now be on site in the outdoor dining area. An

important part of this determination is the details in the shared parking agreement, which should be provided to the Planning Commission for their evaluation of the Special Land Use. Lastly, a description of how the outdoor area will operate and be managed needs to be provided to ensure that the operation of the outdoor dining meets the Special Land Use criteria.

A summary of the comments in the review follows:

Special Land Use

- 1) Planning Commission to consider Director of Public Safety report.
- 2) Revise plans to remove the patio from the Starkweather right-of-way.
- 3) Planning Commission to discuss the addition of landscape planters along the west side of the dining enclosure fence.
- 4) Will the current hours of operation for the restaurant also apply to the outdoor dining area?
- 5) Will the outdoor area be used only for patrons eating a meal, or will patrons only using the bar also be able to sit outside?
- 6) Is the applicant proposing any outdoor speakers or music?
- 7) How will the outdoor area be managed to minimize disturbance to nearby residents?

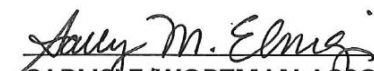
Parking

- 1) Provide “usable” floor area, per the Zoning Ordinance definition of “usable floor area.”
- 2) Include 4-season dining area in the “usable” floor area figure.
- 3) Applicant to confirm that maximum occupancy figure includes the 4-season dining area.
- 4) Applicant to provide shared parking agreement to confirm how parking lot is shared between buildings.
- 5) Narrow proposed outdoor dining so that the width of the adjoining parking space is at least 9-feet wide.
- 6) Modify plans to indicate 8-foot-wide barrier-free parking space, and 8-foot wide aisle.

Recommended Conditions:

If the applicant provides, and the Planning Commission is satisfied with, the additional information regarding the Special Land Use, we suggest the Commissioner’s consider the following conditions to any approval motion:

- 1) Condition approval, if granted, on the applicant confirming/obtaining City Commission approval for the 4-season structure and parking space location in the Starkweather right-of-way.
- 2) Condition approval on the plans being revised to locate the new outdoor patio out of the Starkweather right-of-way, and out of the adjacent parking space.


CARLISLE/WORTMAN ASSOC., INC.
Sally M. Elmiger, AICP, LEED AP
Principal

cc: Greta Bolhuis
Marleta Barr



City of Plymouth Planning Commission March 20, 2024 - Old Village Engagement Session

Survey Results

As part of the process for updating the Master Plan, a public engagement session was held by the Planning Commission on March 20, 2024 at Meridian Coffee in Old Village. Members of the Planning Commission present were Chair Karen Sisolak, Commissioners Joe Hawthorne, Hollie Saraswat and Latricia Horstman. Also present was Planning and Community Development Director Greta Bolhuis.

The Planning Commissioners gave a presentation providing an overview of what the Master Plan is and the process for updating it. Other topics reviewed included:

- Desirable Characteristics for Old Village
- Housing Stock and Diversity
- Accessory Dwelling Units
- City Character
- Form-Based Codes
- Future Land Use

The presentation included questions the Planning Commission had on each topic for the community. At the end of the session the participants were asked to complete an online survey using a QR code that was available online until midnight. Paper copies were available at the session.

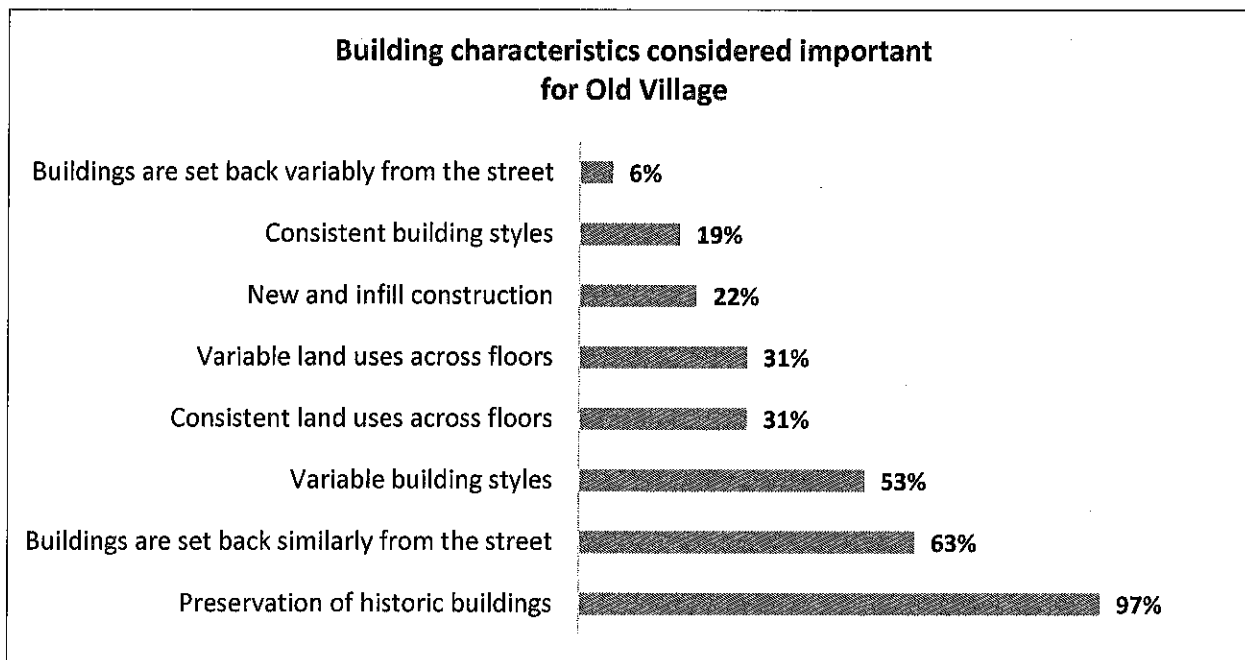
Attendees were asked to sign in when arriving at the session. Based on the sign-in sheets, 31 individuals attended the session. 32 surveys were completed. 31 individuals completed the online survey. 1 individual returned a paper survey. The following provides the age information provided by the respondents.

Age Range	Surveys Completed
18 to 34 years	3
35 to 49 years	10
50 to 64 years	15
65 and over	4
Total:	32

QUESTIONS 1 & 2: Desirable Characteristics for Old Village

Question 1 asked respondents to identify building characteristics they considered important for Old Village. They were provided a list of 8 characteristics to consider. The intent of this question is to verify characteristics identified as important in the 2016 survey have not changed.

Of the 32 responses, all but 1 respondent identified *preservation of historic buildings* as important. Having *buildings set back similarly from the street* and *variable building styles* were also identified as important by a majority of the respondents. These characteristics can also be identified as important when considering the opposite characteristics, *buildings set back variably from the street* and *consistent building styles* had only 6% and 19% of respondents identify them as important. Land uses across floors was tied at 31% which may indicate a lack of importance from the public.



Question 1 also provided the respondents the opportunity to list any other important characteristics. The following were the responses to this open-ended question.

Other Important Characteristics Listed
Explorability
Proportion of dwellings built are in keeping with the other houses/buildings - please stop allowing these bloated McMansions that dwarf the homes around them
I like the small parking lots and on-street parking. Big parking lots are not attractive.
Evolving in a pro-urban manner (adding density, diversity)
Building styles not being too modern and oversized compared their neighboring buildings.
Consistent sizing!
Maintenance of and expansion of parks and trees
Building sized appropriately for lots,

Question 2 asked respondents to please rank a list of pedestrian amenities in order of importance to you from 1 = Highest Priority to 8 = Lowest Priority. This question was asked to identify the level of importance of the pedestrian amenities for Old Village.

The results in the table below provide the average of the responses for each amenity. Also listed are the times each amenity was identified by a participant as their highest priority and lowest priority.

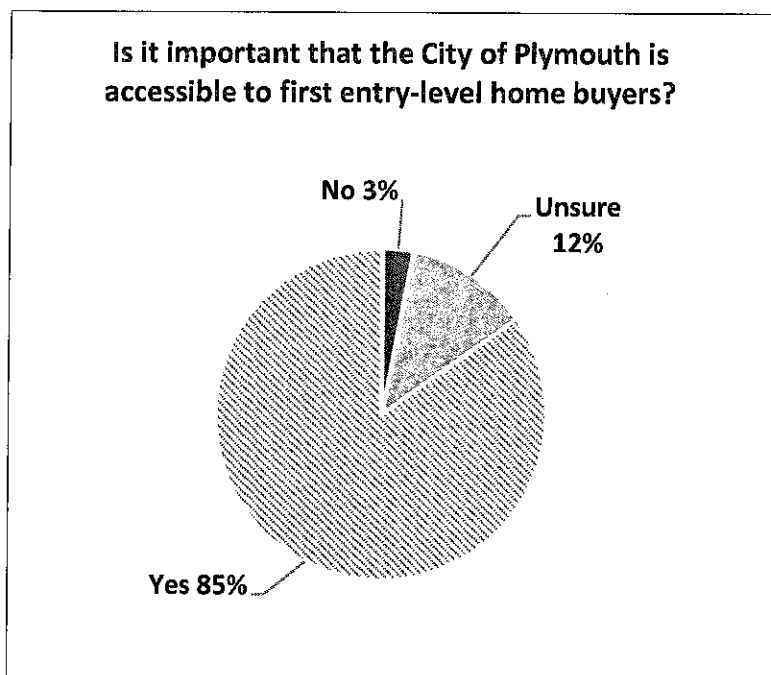
15 of the 32 respondents (47%) identified signalized pedestrian crossings as their highest priority.

11 of the 32 respondents (34%) identified drinking fountains as their lowest priority.

Pedestrian Amenities	Average of Responses	Rank Based on Average	Times Identified as Highest Priority	Times Identified as Lowest Priority
Signalized Pedestrian Crossings	2.56	Highest	15	2
Planters	3.25		4	2
Trash Cans	3.59		2	1
Benches	3.72		1	1
Bike Lanes	4.34		6	5
Bike Racks	5.09		0	2
Directional Signage	5.47		1	6
Drinking Fountains	5.94	Lowest	1	11

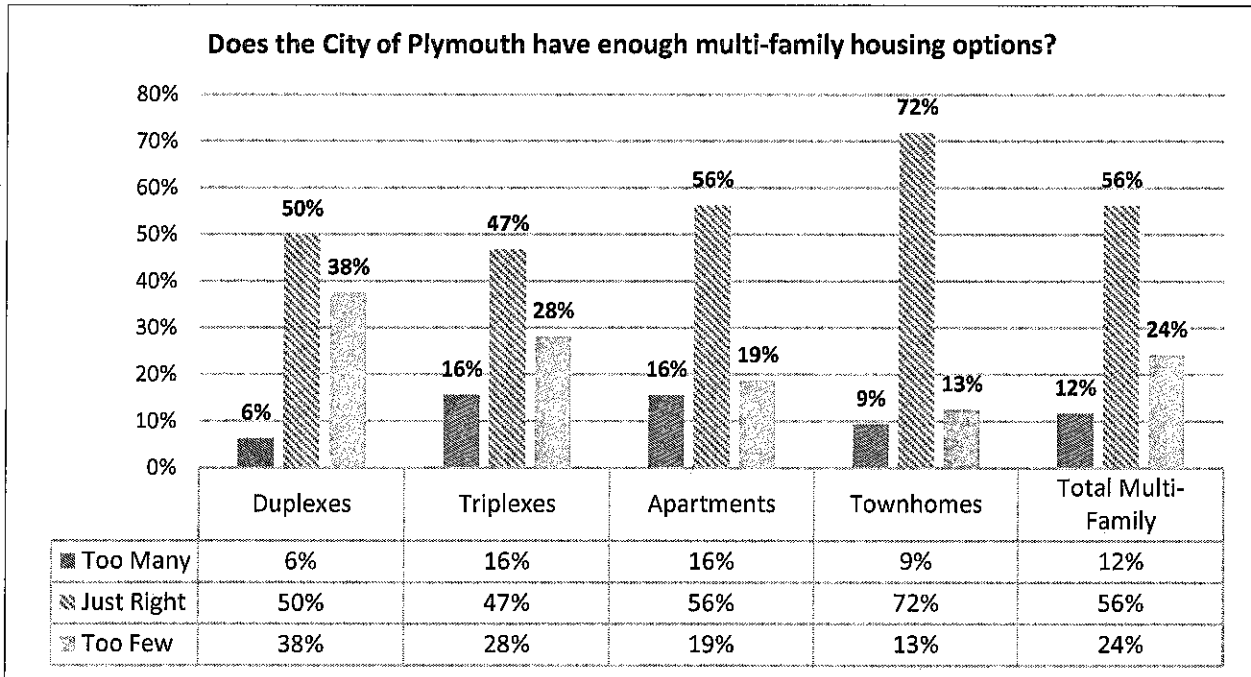
QUESTIONS 3, 4 & 5: Housing Stock and Diversity

For questions 3, 4 and 5, respondents were asked to answer the questions considering housing city-wide. **Question 3** asked if they consider it important that the City of Plymouth is accessible to first time home buyers or entry-level home buyers.



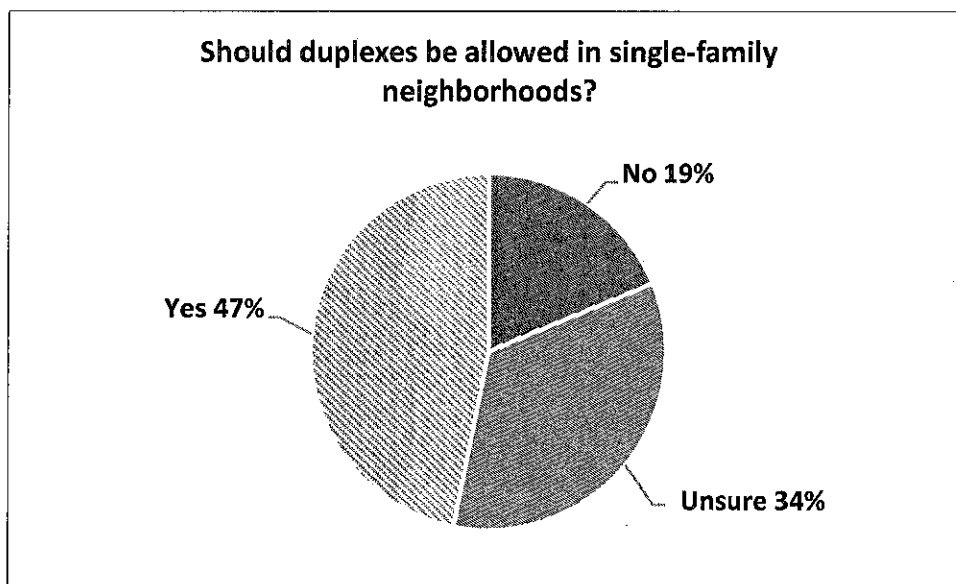
85% of the respondents consider having housing stock accessible to entry-level home buyers important.

Question 4 asked respondents to consider if the City of Plymouth has enough of specific types of multi-family housing (duplexes, triplexes, apartments, townhomes). The intent of this question was to solicit the opinion of respondents on the city's need to increase specific types (duplexes, triplexes, apartments, townhomes) of multi-family housing.



While the previous question did ask about duplexes, **question 5** was specifically targeted to solicit the opinion of respondents on allowing duplexes in single-family districts.

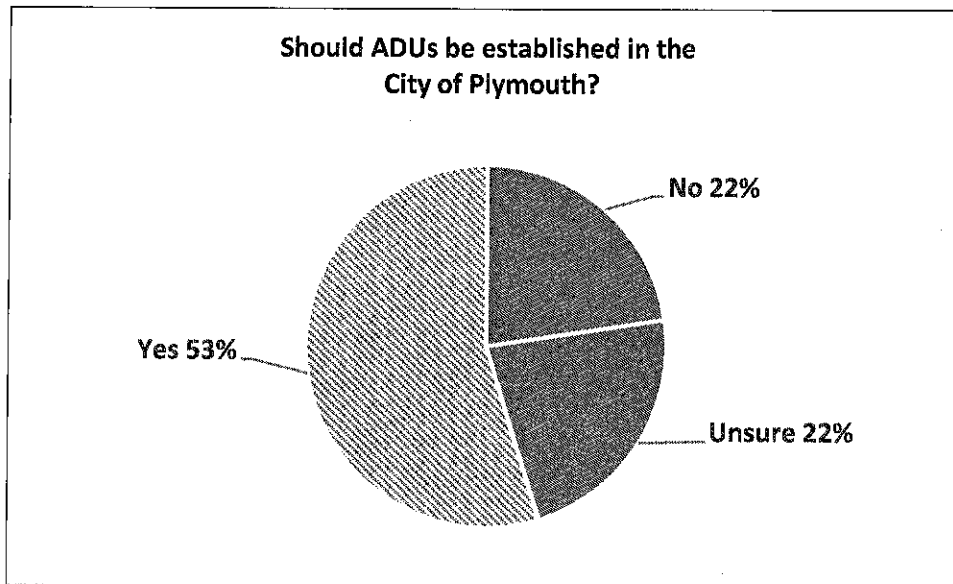
Given the high percentage of unsure responses, it is difficult to make a conclusion on this question. More information with additional examples should be considered for future public engagement sessions.



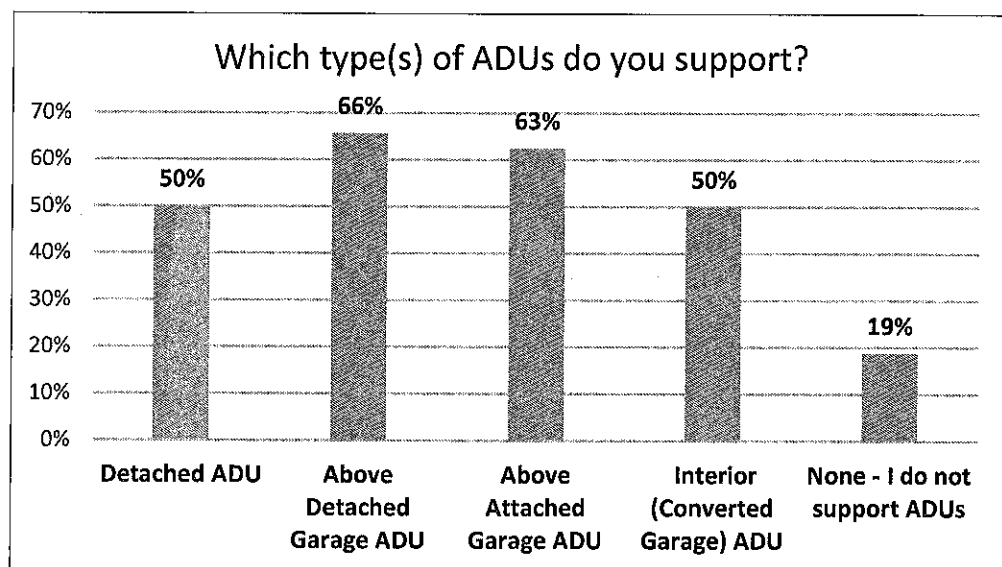
QUESTIONS 6, 7 & 8: Accessory Dwelling Units (ADUs)

A definition of an ADU was provided as part of the presentation at the engagement session which included pictures and graphical representations of various configurations. Questions were asked during the presentation to clarify what is different with an ADU than what is allowed today in accessory buildings.

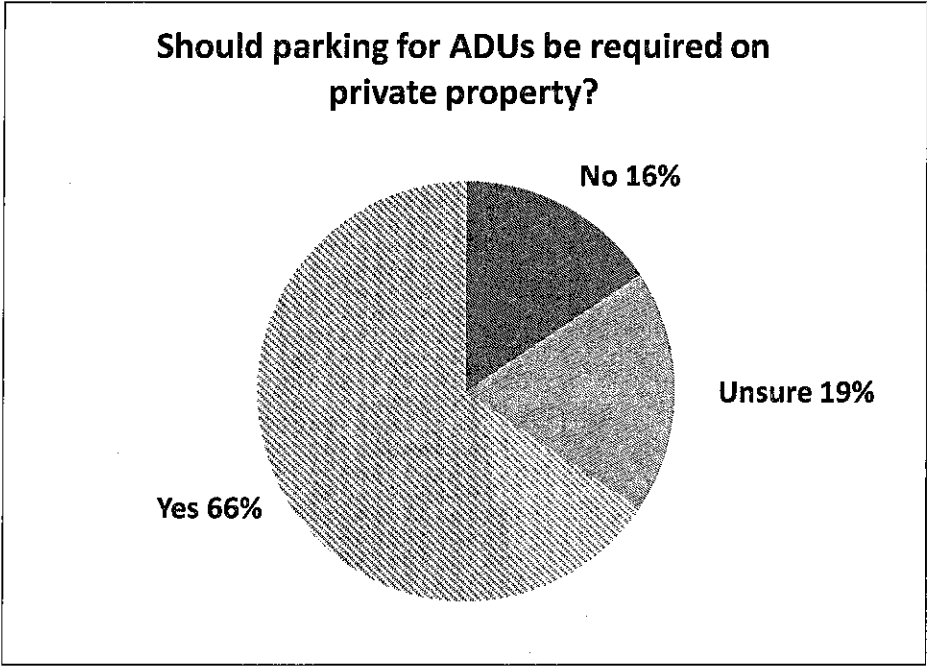
Question 6 asked the participants if they would support allowing ADUs to be established in the City of Plymouth.



Question 7 asked participants to identify which types of ADUs they would support in the City of Plymouth. The graphic below was provided as part of the survey. The options they were asked to consider included Detached ADU, Above Detached Garage ADU (not pictured), Above Attached Garage ADU, Interior (Converted Garage) ADU, and None – I do not support ADUs.



Question 8 Asked the participants for their opinion on requiring parking for ADUs on private property.



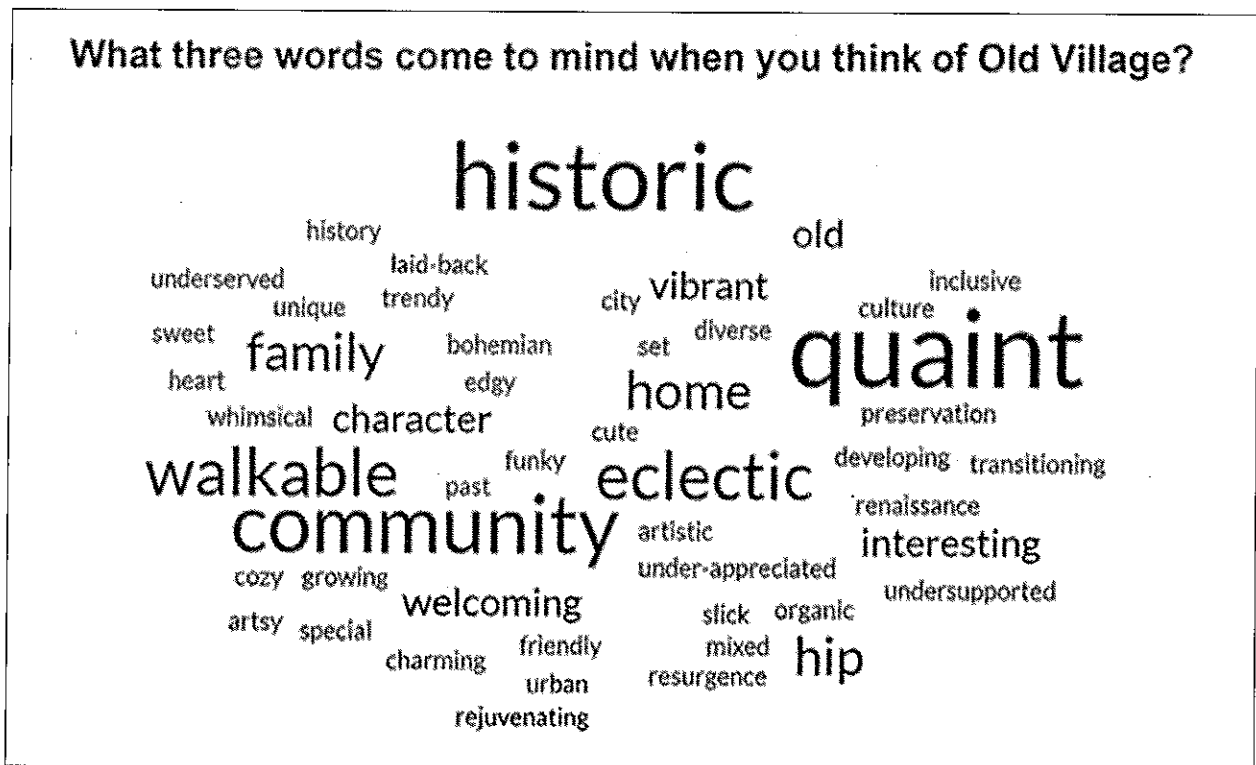
QUESTIONS 9 to 13: City Character

The character of the city was defined during the presentation as the uniqueness that sets us apart from other communities. Examples of walkability, our business districts, neighborhoods, and parks were given as examples.

Question 9 asked the participants to provide three words that come to mind when they think of Old Village. While this does not provide quantitative data, it does provide insight to how the participants perceive Old Village. The following words were the responses received.

diverse, growing,	Walkable cute history	Quaint, Edgy, Historic
Set in the past, whimsical	Organic walkable urban	Hip, trendy and resurgence
History Culture Community	quaint, cozy, old	transitioning, laid-back, welcoming
Unique, family historic	Character, inclusive, Bohemian	Quaint, rejuvenating, undersupported (by city)
History, Community, Heart	Slick hip old	Eclectic, walkable, underserved
Eclectic, charming, developing	Eclectic Vibrant Quaint	Quaint, fun, community
Quaint, friendly, and interesting	Hip artsy community	Walkable, home, engaged
Preservation, family, community	Home Sweet Home	History, up and coming, family
Eclectic under-appreciated artistic	Funky, mixed, interesting	Charmingly Old-Fashioned, serene, Vibrant.
Quaint, welcoming, special	Character, Historical, Renaissance	

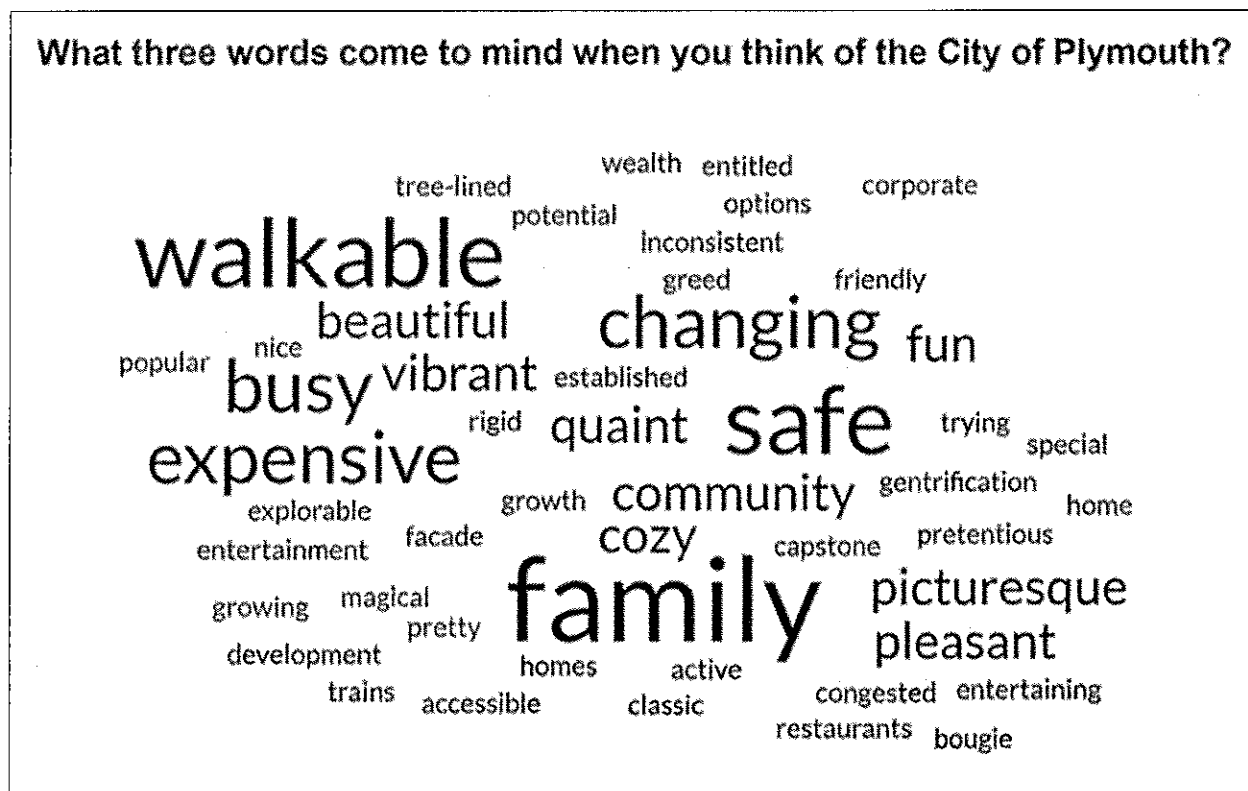
The following word cloud was generated using the responses provided. Note that 4 participants responded with "history", 2 responded with "historic" and 1 responded with "Historical". To reflect the weight more accurately, all 7 were entered as "historic".



Question 10 asked the participants to provide three words that come to mind when they think of the City of Plymouth. Again, the intent was to gain a different perspective of how the participants perceive the City of Plymouth. The following words were the responses received.

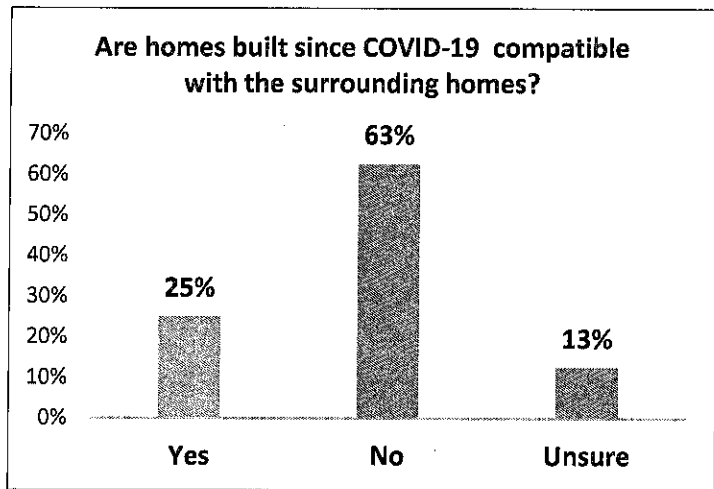
cozy, busy, gentrification	Pleasant homes trains	Upscale, Trendy, Social
Family friendly magical tree-lined	Has more potential	Quaint, vivacious and warmth
Community Development Growth	family, fun, inconsistent	picturesque, walkable, vibrant
Family, explorable, expensive	Wealth, facade, rigid.	Safe, walkable, attractive
Capstone, congested, pretentious	Home nice safe	Changing, busy, beautiful
Active, restaurants, entertaining	Options Family Entertainment	Quaint, community, beautiful
Established, growing, and fun	Walkable safe changing	Exclusive, downtown, changing
Corporate, greed, trying	Vibrant, Safe, Accessible	Hip, convenient, cozy
Bougie busy expensive	Pleasant, pretty, popular	Walkable, Charming, Picturesque
Family , classic , special	Entitled, Expensive, Overfunded	

The following word cloud was generated using the responses provided.

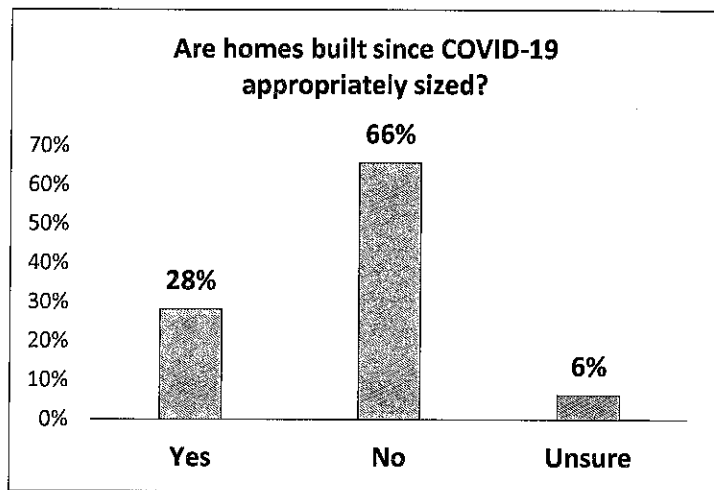


Questions 11, 12 and 13 ask about the character of the city related to changes relating to home size and compatibility of new construction in the city.

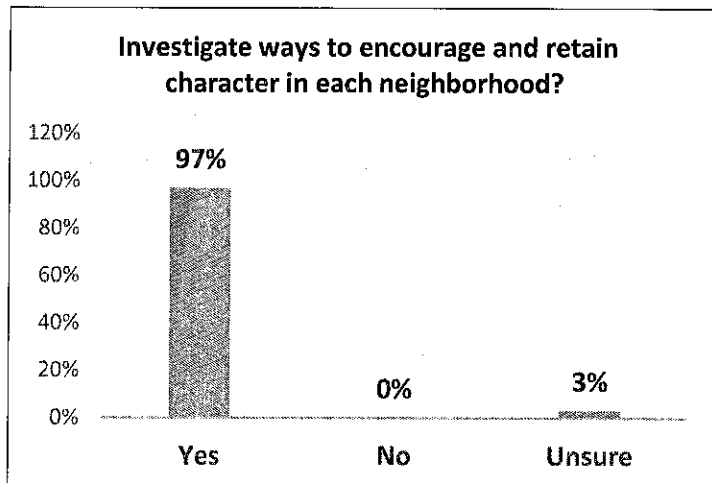
Question 11 asked participants if homes built since COVID-19 (March 2020) compatible with the surrounding homes.



Question 12 asked participants if homes built since COVID-19 (March 2020) are appropriately sized.



Question 13 asked participants if the Planning Commission should investigate possible ways to encourage and retain character in each neighborhood.



QUESTIONS 14 & 15: Form-Based Codes (FBC)

As FBCs is a complex topic, the presentation provided background information on Euclidean Zoning and how FBCs will not change what we regulate but change the focus to ensure that what is built is what the city wants.

The presentation also informed the public that a “test” FBC zoning district was research in 2023. It was recommended as part of this research that as with any major zoning change, the businesses and residents should be educated as to what a form based code is, informed of proposed changes, and consulted as to whether those changes are appropriate.

The group was informed that implementation is at least 2 years out, and further discussions with the public will be required prior to beginning the implementation of FBCs. The following 2 questions were asked to identify the interest by the participants in learning more about FBCs to plan for the future public engagement on the topic.

Are you interested in learning more about Form-Based Codes?	
Yes	88%
No	3%
Maybe	9%

If yes, when?	
2024	66%
2025	13%
When the work begins	22%

QUESTIONS 16 to 19: Engagement Session Feedback

Questions 16, 17 and 18 asked the participants to give feedback on the engagement session with regards to the information provided, length of the sessions, and obtaining answers to their questions. Summary of the responses are below.

Did you find the engagement session informative?	
Yes	97%
No	

How did you feel about the length of the session?	
Too Short	6%
Just Right	91%
Too Long	

Did your questions get answered?	
Yes	88%
No	

Question 19 provided the respondents the opportunity to provide comments or suggestions on the engagement session. The following were the responses to this open-ended question.

Any comments or suggestions?
Great and very informative presentation
Thanks! We need more of this as long as the feedback is used.
Thank you for having this meeting to share the information. Some of Old Village businesses off the main streets don't have great curb appeal. Is there anything we can do to encourage them to tidy them up?
Great and informative meeting.
Thank you for coming to Old Village!
I like having meetings in local businesses rather than public buildings.
Consider allowing street parking overnight with permits. Streets are an underutilized asset and driveways / curb cuts are getting out of control.
Thank you for a great engagement meeting to ensure all completing the survey understand.
Keep asking for community feedback. This is great!
Keep up the amazing work, I'll send ideas
Thank you for meeting in Old Village. I hope Old Village can receive the attention it has been lacking in my 25 years living here.
Thank you. I'm glad to hear that OVP is getting some attention. It would be nice if the sidewalks, parks and planters could be addressed to create a similar aesthetic to the downtown area.
Define or give examples of duplexes in residential neighborhoods, including photos. I think people might associate duplex with a certain form or scale that they view as incompatible, rather than a subdivided home that is virtually indistinguishable from a single family home. Also, I think there is confusion among my neighbors about what kind of finished space is allowed above a garage currently vs the idea of allowing ADUs. I was able to get a clear understanding by asking a few clarifying questions. With ADUs, I support them but fear they could encourage more lot coverage and impermeable surfaces (for parking).
Appreciate your time! Happy to be included
I'm so proud of this team!



City of Plymouth Planning Commission April 24, 2024 - Public Engagement Session

Survey Results

As part of the process for updating the Master Plan, a public engagement session was held by the Planning Commission on April 24, 2024 at the Plymouth Cultural Center. This was the second of two planned engagement sessions. Members of the Planning Commission present were Chair Karen Sisolak, Commissioners Joe Hawthorne, Hollie Saraswat and Latricia Horstman. Also present was Planning and Community Development Director Greta Bolhuis.

The Planning Commissioners gave a presentation providing an overview of what the Master Plan is and the process for updating it. Other topics reviewed included:

- Desirable Characteristics for Old Village and Downtown
- Housing Stock and Diversity
- Accessory Dwelling Units
- City Character
- Form-Based Codes
- Future Land Use

The presentation included questions the Planning Commission had on each topic for the community. At the end of the session the participants were asked to complete an online survey using a QR code that was available online until midnight. Paper copies were available at the session.

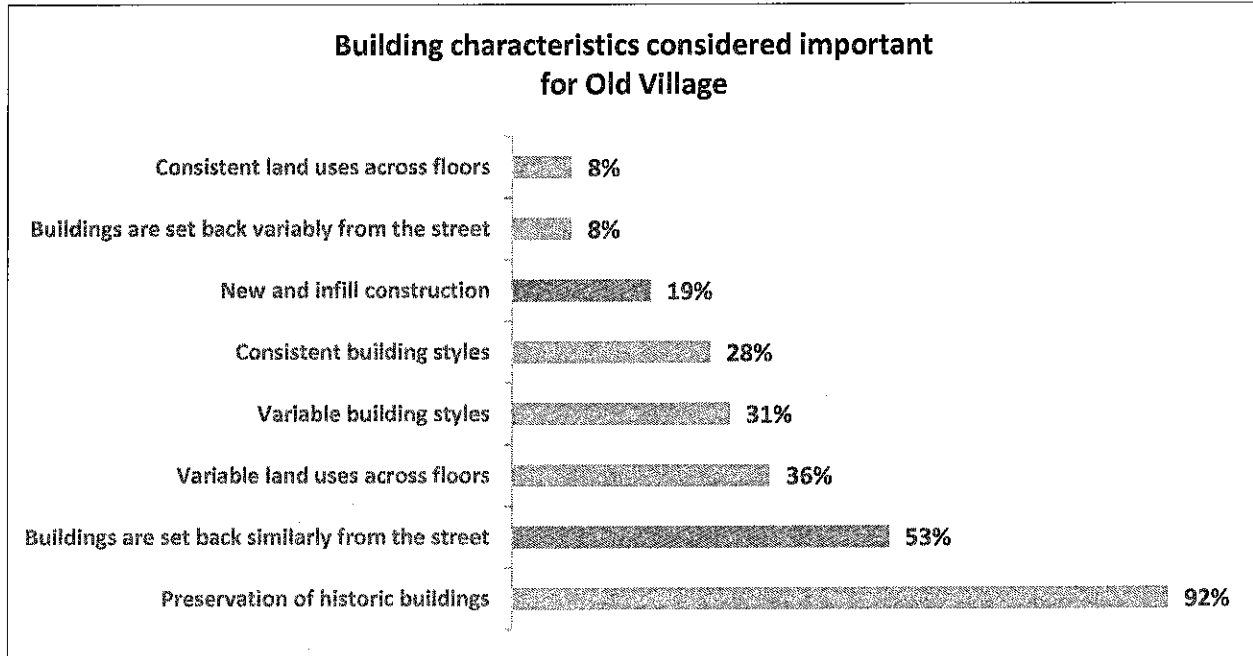
Attendees were asked to sign in when arriving at the session. Based on the sign-in sheets, 39 individuals attended the session, and 36 surveys were completed. The following provides the age information provided by the respondents.

Age Range	Surveys Completed
18 to 34 years	1
35 to 49 years	5
50 to 64 years	13
65 and over	17
Total:	36

QUESTIONS 1 & 2: Desirable Characteristics for Old Village

Question 1 asked respondents to identify building characteristics they considered important for Old Village. They were provided a list of 8 characteristics to consider. The intent of this question is to verify characteristics identified as important in the 2016 survey have not changed.

Of the 36 responses, all but 3 respondent identified preservation of historic buildings as important. Over half of the respondents considered having buildings set back similarly from the street important.



Question 1 also provided the respondents the opportunity to list any other important characteristics. The following were the responses to this open-ended question.

Other Important Old Village Characteristics Listed
Get rid of parking
Keep the walkability of the area
Don't destroy what's special about it like downtown.
Traffic calming. Bike and walk ability.
Infrastructure that allows for OVA to put on more events.
Open park space
Consider roof top venues
Make more pedestrian friendly
Maintaining a style of new construction that is consistent with the character of the existing older structures.
Character and preservation-focused or new buildings and uses that match the existing feel of the area.
Preservation of trees.
Height variations
Retaining the historic old town feel
Buildings consistent in style with existing historic buildings
Old Village to me is Liberty and south on Starkweather to Farmer. Retain residential (single family) on west side of Starkweather.

Question 2 asked respondents to please rank a list of pedestrian amenities in order of importance to you from 1 = Highest Priority to 8 = Lowest Priority. This question was asked to identify the level of importance of the pedestrian amenities for Old Village.

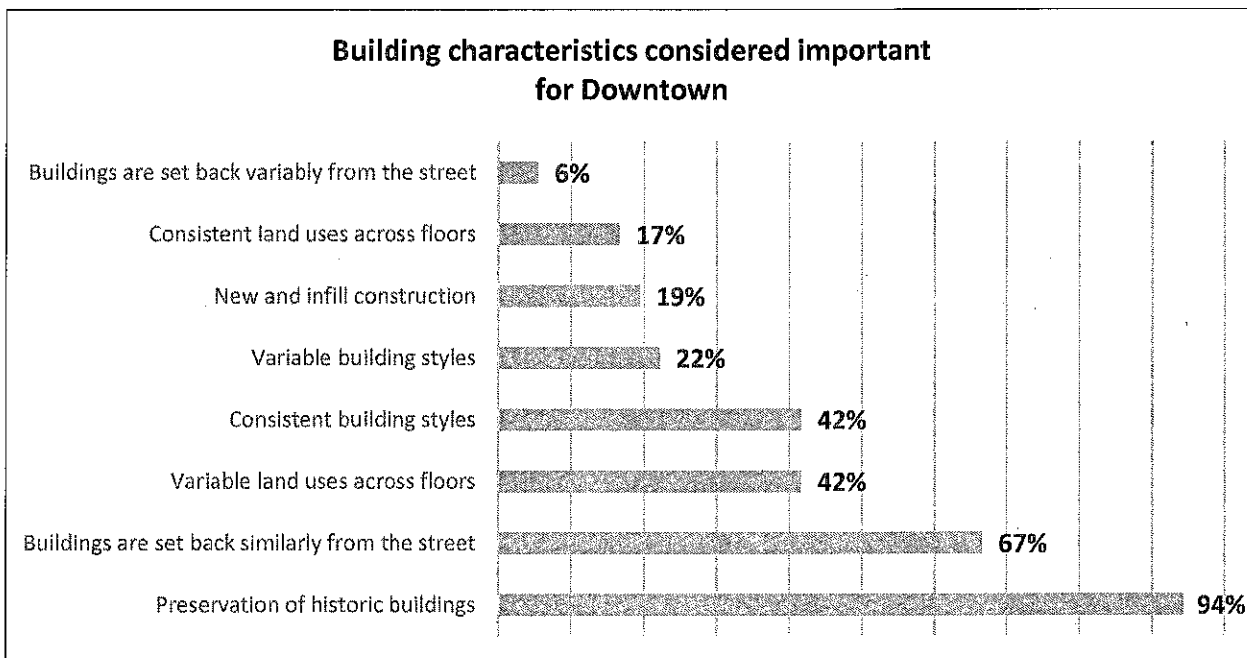
The results in the table below provide the average of the responses for each amenity. Also listed are the times each amenity was identified by a participant as their highest priority and lowest priority. The top 4 pedestrian amenities, while in different order, are consistent with the survey results from the March 20, 2024, engagement session.

Pedestrian Amenities Old Village	Average of Responses	Rank Based on Average	Times Identified as Highest Priority	Times Identified as Lowest Priority
Benches	2.44	Highest	5	1
Trash Cans	2.89		5	0
Planters	2.94		4	0
Signalized Pedestrian Crossings	2.97		5	2
Bike Racks	3.17		2	1
Bike Lanes	3.58		7	6
Directional Signage	4.11		2	7
Drinking Fountains	4.86	Lowest	0	11

QUESTIONS 3 & 4: Desirable Characteristics for Downtown

Question 3 asked respondents to identify building characteristics they considered important for Downtown. They were provided the same list of 8 characteristics to consider as in Question 1 to identify any potential changes from the 2016 survey.

The top 2 characteristics are consistent with the results from Question 1 for Old Village.



Question 3 also provided the respondents the opportunity to list any other important characteristics. The following were the responses to this open-ended question.

Other Important Downtown Characteristics Listed
More density
Keep the walkability of the area
Building size appropriate to lot
The new & expensive pedestrian signs are ridiculous when a timer connected to the traffic light will keep people safe. The flashing, solar, ugly signs actually have the disclaimer "traffic is not required to stop." There was talk about a real stop sign (enforced by law & tickets) to keep kids safe walking to Bird Elementary & West Middle School. I'm grateful for the crossing guards & they could have been paid a real salary with the amount of money wasted on these "improvements." Please follow your own Bigfoot & setback & height rules on new construction. Adding "variances" is not a solution & undercuts the goals in the master plan everyday.
Appropriate home massing (sizes)
Preservation of open spaces
Consider roof top venues
Retaining the characteristic and historic buildings. Maintaining integrity of various neighborhoods. Avoiding increased density. Note: I have honestly never seen anyone using the bike lanes. While I support bikes, I don't think the bike lanes are being used. I also don't know that the signaled signs have improved or changed pedestrian flow for main intersections which are not near schools.
Height variations
Variety of businesses
Building styles that complement the historic buildings
A contraction of liquor licenses in the DDA and a contraction 10:30 pm on closings in the DDA.
Size of homes managed. No duplexes, no rental properties
trees, green space
Keep the historic charm/facades and architecture

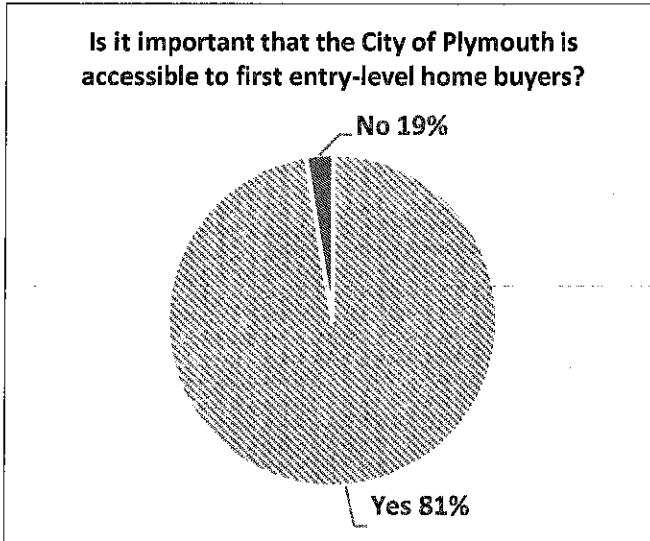
Question 4 asked respondents to please rank a list of pedestrian amenities in order of importance to you from 1 = Highest Priority to 8 = Lowest Priority. This question was asked to identify the level of importance of the pedestrian amenities for Downtown.

The results in the table below provide the average of the responses for each amenity. Also listed are the times each amenity was identified by a participant as their highest priority and lowest priority.

Pedestrian Amenities Downtown	Average of Responses	Rank Based on Average	Times Identified as Highest Priority	Times Identified as Lowest Priority
Benches	2.58	Highest	2	0
Trash Cans	2.86		3	1
Signalized Pedestrian Crossings	3.06		8	1
Planters	3.36		4	1
Bike Racks	3.61		2	0
Bike Lanes	3.61		7	8
Directional Signage	4.56		1	5
Drinking Fountains	4.58	Lowest	2	12

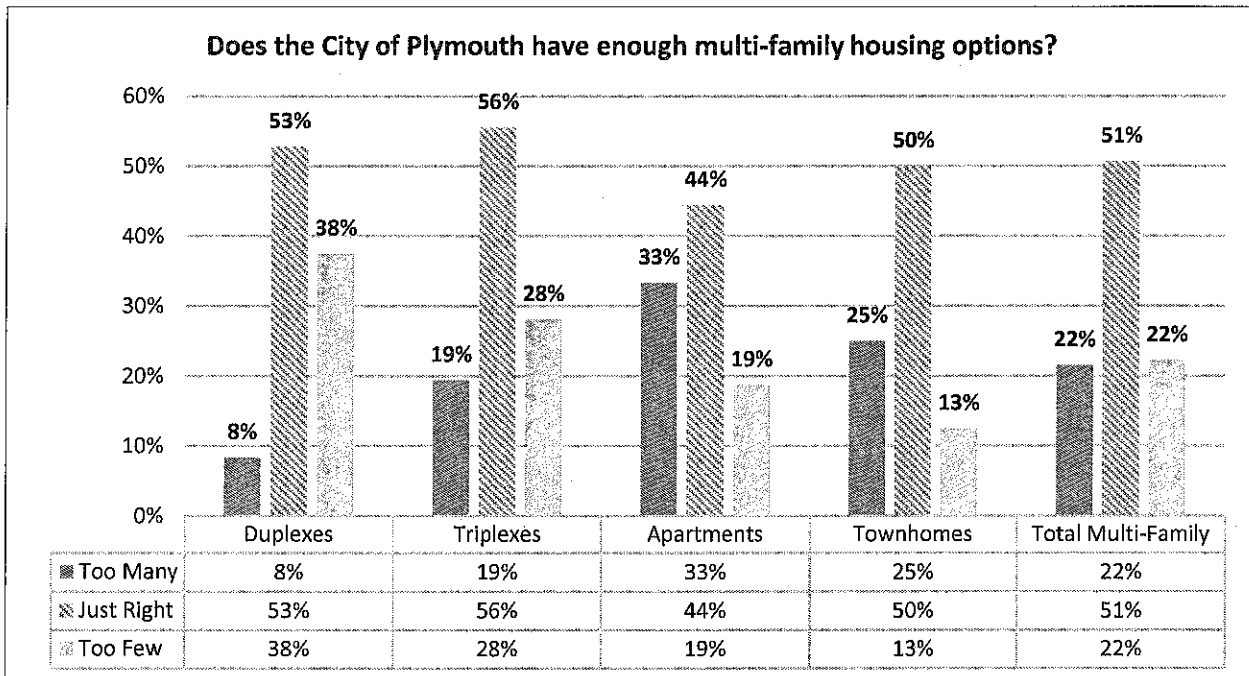
QUESTIONS 5, 6 & 7: City-wide Housing Stock and Diversity

Question 5 asked if they consider it important that the City of Plymouth is accessible to first time home buyers or entry-level home buyers.



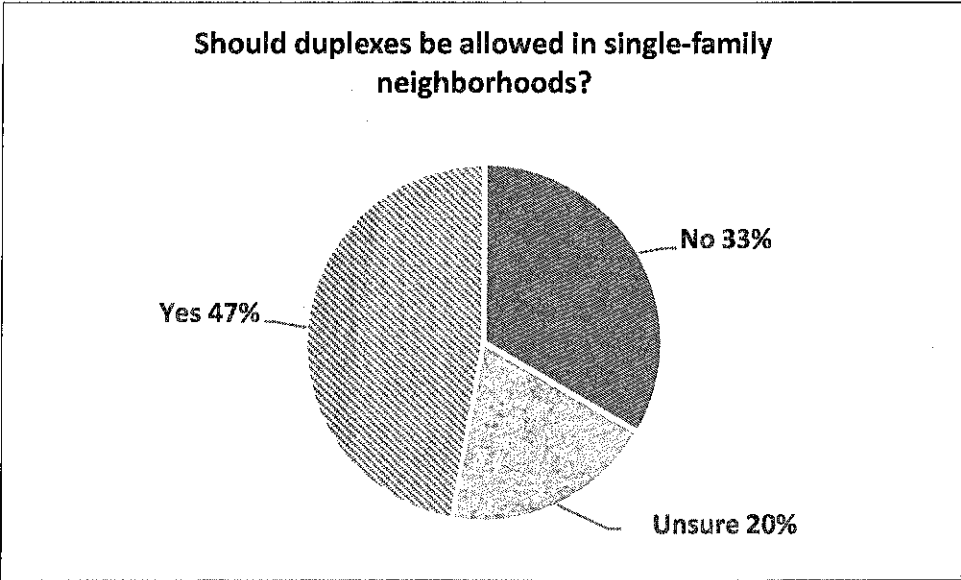
85% of the respondents consider having housing stock accessible to entry-level home buyers important.

Question 6 asked respondents to consider if the City of Plymouth has enough of specific types of multi-family housing (duplexes, triplexes, apartments, townhomes). The intent of this question was to solicit the opinion of respondents on the city's need to increase specific types (duplexes, triplexes, apartments, townhomes) of multi-family housing.



While the previous question did ask about duplexes, **question 7** was specifically targeted to solicit the opinion of respondents on allowing duplexes in single-family districts.

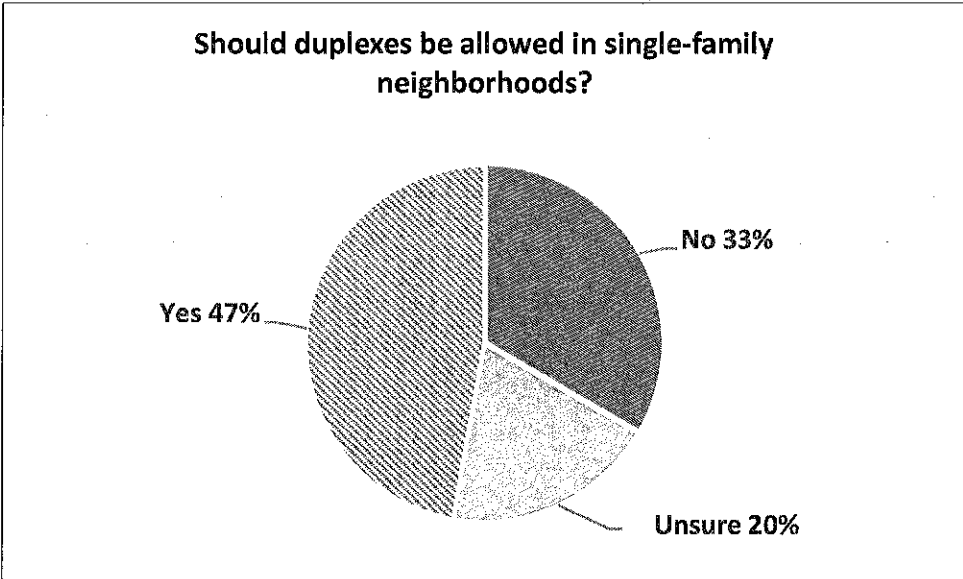
Given the high percentage of unsure responses, it is difficult to make a conclusion on this question.



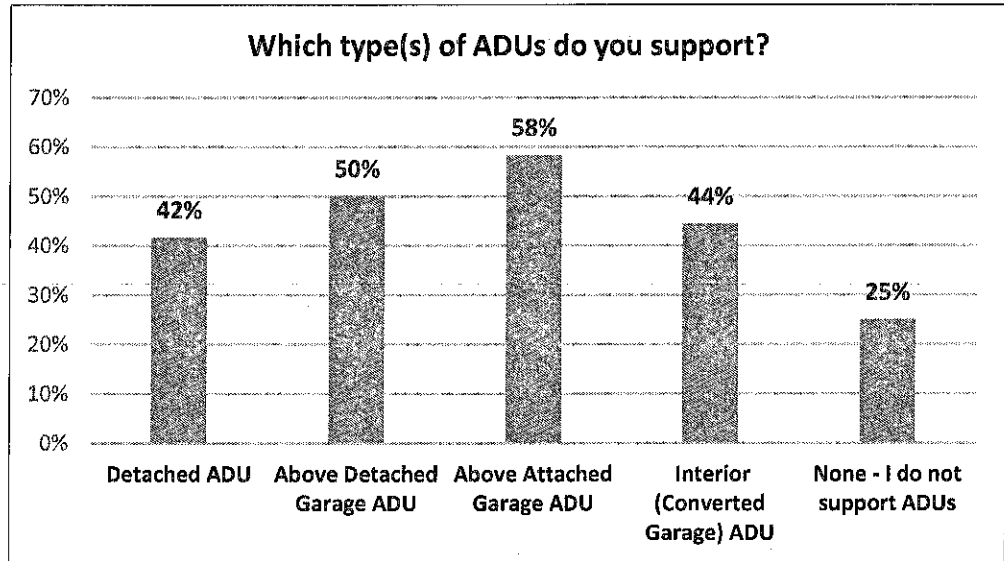
QUESTIONS 8, 9 & 10: Accessory Dwelling Units (ADUs)

A definition of an ADU was provided as part of the presentation at the engagement session which included pictures and graphical representations of various configurations. The presentation included a description of what is allowed today in accessory buildings as there were questions on this in the first engagement session.

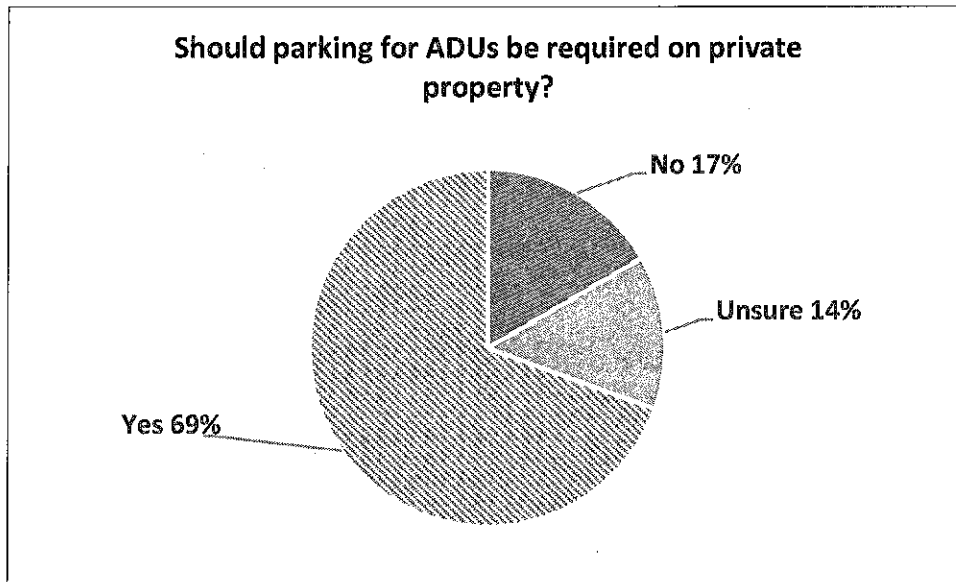
Question 8 asked the participants if they would support allowing ADUs to be established in the City of Plymouth.



Question 9 asked participants to identify which types of ADUs they would support in the City of Plymouth. The graphic below was provided as part of the survey. The options they were asked to consider included Detached ADU, Above Detached Garage ADU (not pictured), Above Attached Garage ADU, Interior (Converted Garage) ADU, and None – I do not support ADUs.



Question 10 Asked the participants for their opinion on requiring parking for ADUs on private property.



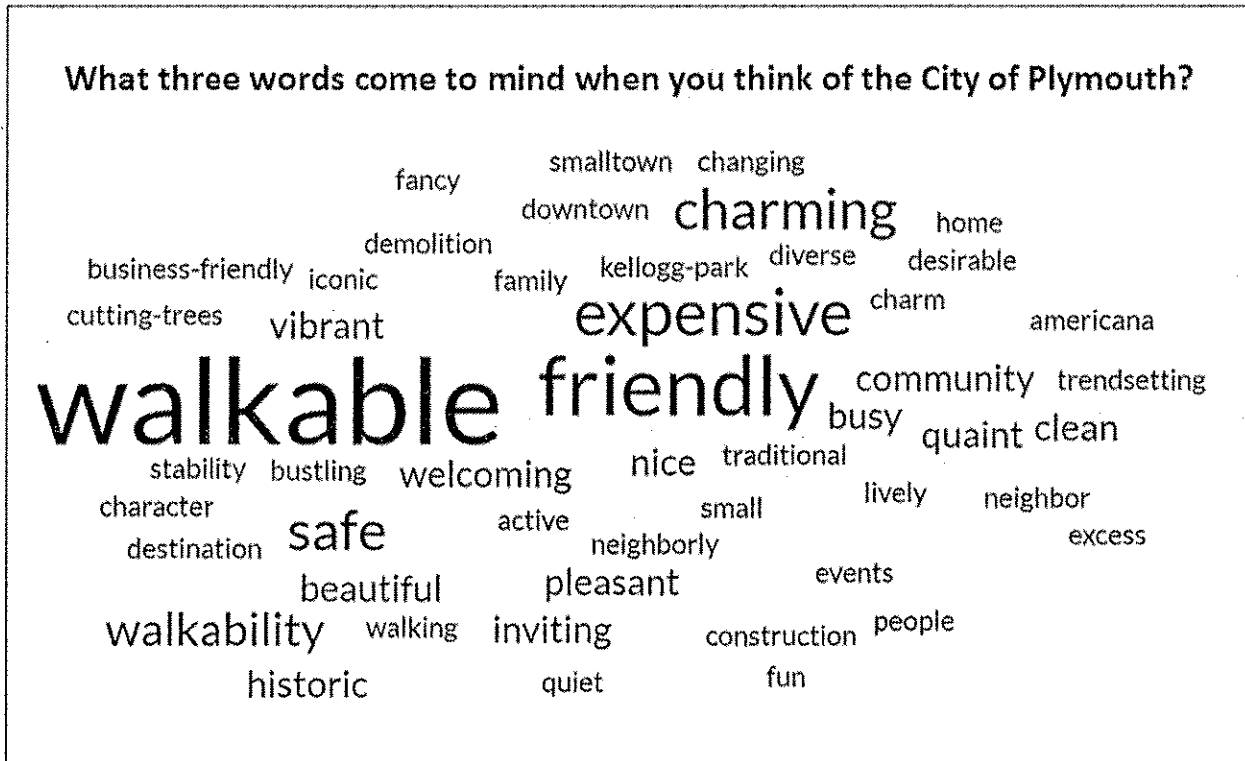
QUESTIONS 11 to 14: City Character

The character of the city was defined during the presentation as the uniqueness that sets us apart from other communities. Examples of walkability, our business districts, neighborhoods, and parks were given as examples.

Question 11 asked the participants to provide three words that come to mind when they think of the City of Plymouth. While this does not provide quantitative data, it does provide insight to how the participants perceive City of Plymouth. The respondents provided the following words. Only the adjectives are listed if the response was in the form of a statement.

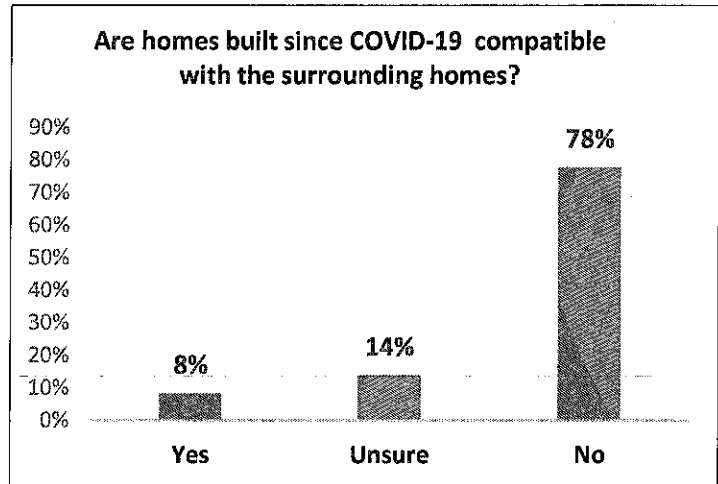
What three words come to mind when you think of the City of Plymouth?		
Walkability iconic americana	Home, nice community	Demolition, Construction, Excess
Walkable, quiet, desirable	Nice community	People, walkable, fun
Busy fancy expensive	Small, beautiful, friendly	Kellogg-park, downtown, Historic
Expensive, walkable, neighborly	Quaint, walkable, safe	Expensive, walkable, events
Charming expensive walkable	Pleasant, historic, traditional	Charm, character, walkability, safe
Expensive, friendly, active	Vibrant inviting Safe	Walkability, small town charm
Walkable, friendly, diverse	Walking cutting trees	Quaint, friendly, busy
walkable, lively, business-friendly	Friendly, walkable, pleasant	changing, vibrant, destination
Family, safe, stability	Inviting, bustling, trendsetting	Walkability, access, charm
Charming, welcoming, walkable	Clean, friendly, beautiful	
Neighbor friendly	Clean, safe and welcoming	

The following word cloud was generated using the responses provided.

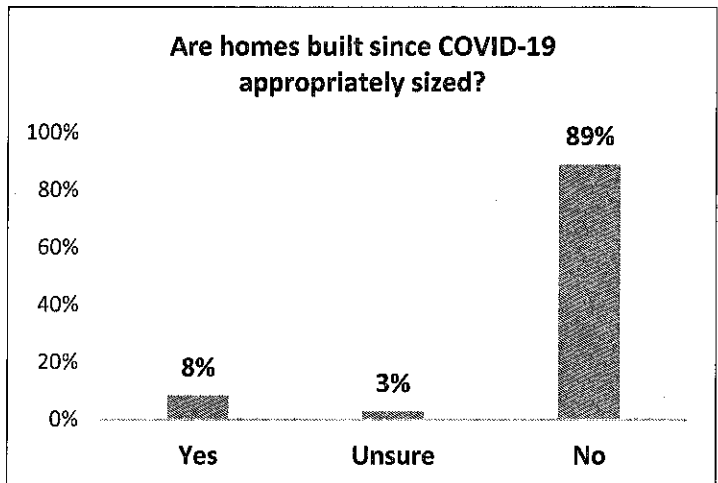


Questions 12, 13 and 14 ask about the character of the city related to changes relating to home size and compatibility of new construction in the city.

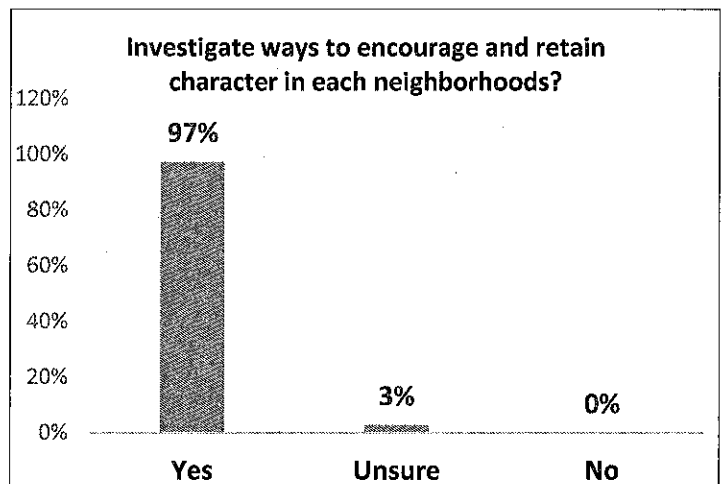
Question 12 asked participants if homes built since COVID-19 (March 2020) compatible with the surrounding homes.



Question 13 asked participants if homes built since COVID-19 (March 2020) are appropriately sized.



Question 14 asked participants if the Planning Commission should investigate ways to encourage and retain character in each neighborhood.



QUESTIONS 15 & 16: Form-Based Codes (FBC)

As FBCs is a complex topic, the presentation provided background information on Euclidean Zoning and how FBCs will not change what we regulate but change the focus to ensure that what is built is what the city wants.

The presentation also informed the public that a “test” FBC zoning district was researched in 2023. It was recommended as part of this research that as with any major zoning change, the businesses and residents should be educated as to what a form based code is, informed of proposed changes, and consulted as to whether those changes are appropriate.

The group was informed that implementation is at least 2 years out, and further discussions with the public will be required prior to beginning the implementation of FBCs. The following 2 questions were asked to identify the interest by the participants in learning more about FBCs to plan for the future public engagement on the topic.

Are you interested in learning more about Form-Based Codes?	
Yes	88%
No	3%
Maybe	9%

If yes, when?	
2024	61%
2025	3%
When the work on it begins	25%
No response	11%

QUESTIONS 17, 18 & 19: Engagement Session Feedback

These questions asked the participants to give feedback on the engagement session with regards to the information provided, length of the sessions, and obtaining answers to their questions. Summary of the responses are below.

Did you find the engagement session informative?	
Yes	92%
No	6%
No Response	2%

How do you feel about the length of the session?	
Too short	6%
Just right	92%
Too long	3%

Did your questions get answered?	
Yes	81%
No	8%
No Response	11%

Question 20 provided the respondents the opportunity to provide comments or suggestions on the engagement session. The following were the responses to this open-ended question.

Any comments or suggestions?
Try to address ARC zoning
Thank you! This was my first planning meeting.
Thanks for this session. Very informative
Great. Additions since first presentation all helped.
The homes being built at present are too big. The old homes were appropriately sized then are knocked down and replaced with structures that fill the lots to the detriment of neighbors
add social districts to the plan
Concerns about oversized building in residential plus proposals for multi family apartment style complex in town, too much
I support multi-family dwellings and ADUs in Plymouth for use of current property owners but not as real-estate/money-making opportunities for offsite landlords.
Thank you for sharing all of this information!
I will send an email.
Well done
Thank you for serving our awesome city
Keep up the good work
Thank you for this presentation and for encouraging input and feedback from the community.
No
I would love a session like this about green spaces and parks.
Thank you for presentation. I think the City needs to think about ways to preserve historical homes and large heritage trees in our neighborhoods. The trees are not being protected is really changing the look and feel of streets. The homes are too big for lots and homes are nearly encroaching in sidewalks. People used to come to Plymouth for the character, but we are losing some of that character with some of the over-development. Serious thought needs to be given to impact of these dense developments.
I liked the location, great presentation and delivery. I appreciate that questions could be asked throughout the presentation.
I liked Scott Lorenzo's suggestion about underground wires. This good for infrastructure but also for esthetics, which seems to fit in with form based codes.
Plymouth Rd from Haggerty to Ann Arbor Rd traffic drives too fast. Is there a way to slow it down?
Great job for all involved. Thank you for your transparency. Members should say what streets they live on.
Thanks for having this
Nothing about keeping, maintaining green space and trees
Great meeting
Thank you! Not enough young people here. Need their input! Can't afford to move as a retired senior. Not enough small, first floor only ranch homes or condos for us. Don't want to move out of Plymouth. Need more smaller homes/units. More you let builders tear down, less available affordable housing.

Hello Plymouth City Commissioners and Planning Commission members,

I would like to suggest that you consider these items.

We need a left turn lane for westbound Ann Arbor Trail at Sheldon. Someone turning left backs up traffic all the way past Evergreen and sometimes to McKinley Street. This backup causes people to speed through Maple Street and Sheridan Street to avoid the bottleneck on Ann Arbor Trail. I know it's not easy but neither was the viaduct over Sheldon road.

The parking problem caused by the 100 unit Tonquish Manor overflows onto Joel and Sheridan EVERY DAY. Those residents can't enjoy their own residential streets. The Christian Science Church rents spaces to the Manor but it's not enough. This is a City zoning enforcement issue.

The Christian Science Church PUD request is an issue that's not going away. Please retain and respect the site's current single family zoning. The area residents clearly do not want the proposed multi-family/attached housing change to the Masterplan. This is a single family residential neighborhood and not a high density area that is being proposed. Perhaps the City needs to buy the property and control what happens there just as they did with the Saxton's building. This turned out well, all things considered.

Foster the beautification of Main Street from Ann Arbor Rd. to Wing Street. Since this is the entry to Plymouth it needs to be more attractive. Make it as nice as Harvey Street between Ann Arbor Trail and Penniman with new street and sidewalk, make it tree lined the entire way. Extend the current street lights all the way to Ann Arbor Road.

Please consider enhancing the lighting throughout all the residential areas. Most of the 'COBRA Head' lights we currently have look like they came out of a Kmart parking lot and the Detroit House of Correction. We can do much better than this. We first need a plan with a vision and then figure out how to fund it. The lighting could reflect each neighborhood's interest, history etc. so they are not a 'one size fits all' approach.

Despite the millage defeat, recreation is important to the residents. However not at all costs. For example, of the 800+ kids that participate in the city organized soccer program only 35% are from the City of Plymouth. The rest are from Plymouth Township and elsewhere. Spending \$5 Million+ on soccer fields for others was a big reason the millage was defeated. A much more economical solution would be to use the PARC practice field area that borders the Cultural Center lot, the football bleachers and the PARC parking lot. I spoke to Steve Anderson and there is space for two soccer fields. Let's make a deal to use these fields. It would be financially prudent and handle a good part, if not all, of the shortfall due to the loss of your other fields previously offered by the schools.

Street sweeping. It is important to keep the streets in all residential areas clean. The city does a great job in the downtown area but the residential areas are not kept clean to the same standard. There are issues with cars parked on the street that block the street sweeper. However if we knew when our area would be swept we'd all make it a point to keep the cars off the streets. Just as you have zones for leaf pickup you could have the same for the street cleaning. We need a schedule like Zone 1 is the first Wednesday or Thursday of each month.

Bury the powerlines whenever possible. All new subdivisions require powerlines to be buried. When we have a chance to bury them with new projects that should be our priority. Sometimes costs are prohibitive and sometimes not. We missed a real opportunity to bury the lines that run through the SAXTONS parking lot and also in front of the new homes on Maple Street. We need higher standards.

I found the presentation about the "13 Ways to Kill Your City" to be informative and inspiring. We cannot afford to sit on our laurels. Plymouth is great, and we need to strive to improve it whenever possible.

Thank you for your service to the community. Please know it is appreciated.

Thank you for your consideration of these items.

RECEIVED

MAY 21 2024

CITY OF PLYMOUTH
PLANNED UNIT DEVELOPMENT APPLICATION

Community Development Department
201 S. Main Street Plymouth, MI 48170
Ph. 734-453-1234 ext. 232
www.plymouthmi.gov

CITY OF PLYMOUTH
COMMUNITY DEVELOPMENT

I. Site/Project Information

Site Address 230 Wilcox Rd.	Current Zoning Classification R-1	Date of Application 5/20/24
--------------------------------	--------------------------------------	--------------------------------

Name of Property Owner Donofrio Wil Cox, LLC	Phone Number 248-470-4836	
Mailing Address P.O. Box 935	Email Address (Required) GKDelta@outlook.com	
City Northville	State MI	Zip Code 48167

II. Applicant and Contact Information

Indicate Who the Applicant Is. If Property Owner, Skip to Section III.	Architect	Developer	Engineer	Lessee
Applicant/Company Name	Phone Number			
Applicant/Company Address	City	State	Zip Code	
Email Address (Required)				

III. Site Plan Designer and Contact Information

Site Plan Designer Company Name TRUE NORTH SURVEYING, INC - MICHAEL Johnson	Phone Number 734-676-7799		
Company Address 23481 GLORIA DRIVE	City BROWNSTOWN	State MI	Zip Code 48183
Registration Number 52457	Expiration Date 6/30/26	Email Address (Required) Tnsurveying@comcast.net	

IV. Type of Project

Commercial
 Industrial
 Mixed Use
 Multi-Family
 Single Family

Preliminary Plan
 PUD Review/Revision
 Final Plan
 PUD Amendment/Extension

V. Historic District

Is this project located in the Historic District?
 Yes No

VI. Description of Project

Restoration and Repurposing of the Wilcox Mill Building.
 Opening up Wilcox Mill Property for Public Enjoyment through
 Commercial development. Intended uses are general retail business

VII. Fee Schedule

Fee Item	Fee	Quantity	Total
PUD Preliminary Plan (initial review only)	\$1,500.00		
PUD per acre fee	\$75.00		
Rezoning	\$800.00		
Condominium project	\$500.00		
Subsequent PUD review/revision	\$1,000.00		
Final PUD	\$1,100.00		
Extension of PUD approval	\$850.00		
Amendment to approved PUD	\$850.00		
Special PC meeting	\$900.00		
TOTAL FEE			

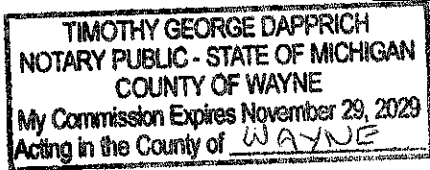
VIII. Applicant Signature

Signature of Applicant 	Date 10-May-2024
---	---------------------

IX. Property Owner Signature


Signature of Property Owner 	Date 10-May-2024
--	---------------------

Subscribed and sworn before me this 10th day of MAY, 2024.



Notary Public: Timothy George Dapprich

My Commission expires: 11/29/29



For Office Use Only

	YES/DATE	NO	N/A
1. Pre-Application Meeting			
2. Digital Copy of Application Package			
3. Public Hearing Notice			
4. CWA Review			
5. Municipal Services Review			
6. Fire Department Review			
7. Engineering Review			

WILCOX MILL RESTORATION

PUD SUBMISSION PACKAGE

MAY 2024

DONOFRIO WIL COX, LLC

1156 BEECH ST.
PLYMOUTH, MI 48170

Wilcox Mill Restoration – PUD Narrative

The Wilcox Mill restoration project will focus on the restoration and adaptive reuse of the historic Wilcox Mill building. The Wilcox Mill was built in 1923, one year after the completed construction of the Phoenix Mill, just up the road on Hines Drive. The Mill building was constructed by the Ford Motor Company and was part of Henry Ford's Village Industry Mill buildings. The building was instrumental in making durable tooling, bolt thread taps, for Model T's and Model A's, shipping the tools down to the mighty Rouge River factory. This small factory helped "put the world on wheels" and was part of our country's "Arsenal of Democracy," making parts for World War II, and is an important part of our local history.

The Wilcox Mill was previously owned by Wayne County from the late 1940's until late 2023. The building stored vehicles for Wayne County and then holiday decorations for the annual Hines Drive light parade. The building has been boarded up for decades. Water penetration and lack of maintenance/upkeep has caused significant damage to the building. Water, gas, and electric were all cut off from the building many years ago. Without restoration, the building will continue to deteriorate, eventually becoming unsalvageable.

The aim of the project is to seal, secure, and restore the Mill building, putting it back into productive use for the community. The primary focus will be on ensuring the proper restoration of the building, including saving and restoring the original steel sash window frames and restoring the Mill Road facing façade, bringing back the original look.

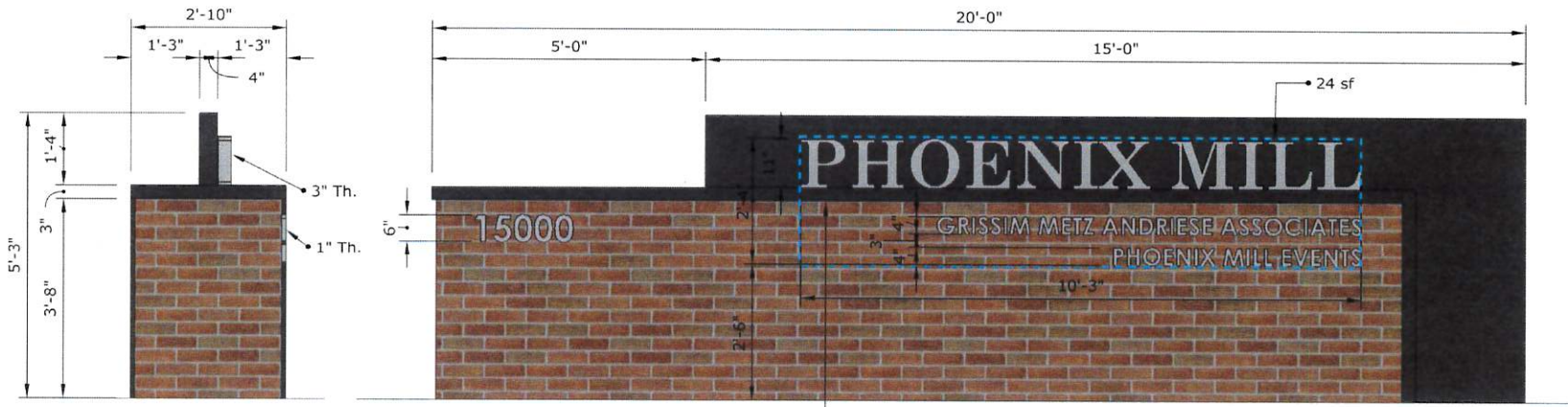
In order to maximize the local community benefit and take advantage of the Mill's location adjacent to Hines Park, the targeted use of the building will be a Coffee Shop, with a potential retail front on the lower level aimed at enhancing the Park experience. The back side, with an opened-up spillway, will seamlessly integrate the Wilcox parcel land to the surrounding park space, providing park benches and cleaned-up green spaces to explore. Given the location adjacent to the Hines Park space, a PUD is imperative to allow maximum use and community benefit.

RENDERING



MONUMENT SIGN

*SIGN WILL BE INDENTICAL TO THE PHOENIX MILL SIGN (SHOWN), EXCEPT WITH 'WILCOX MILL'
THIS WILL KEEP CONSISTENCY BETWEEN THE RESTORED FORD MILL BUILDINGS IN OUR
PLYMOUTH COMMUNITY.



LEFT SIDE ELEVATION

FRONT ELEVATION

→ EXTRUDED ALUMINUM LETTERS

→ PAINTED ALUMINUM

→ BRICK TO MATCH BUILDING



Scale: 1/2" = 1'-0"



Phoenix Mill
Plymouth Township, Michigan

Entrance Monument Sign
Perspective Rendering
05.24.2022

GRISSIM
METZ ASSOCIATES
ANDRIESE

Original Wilcox Mill - Front



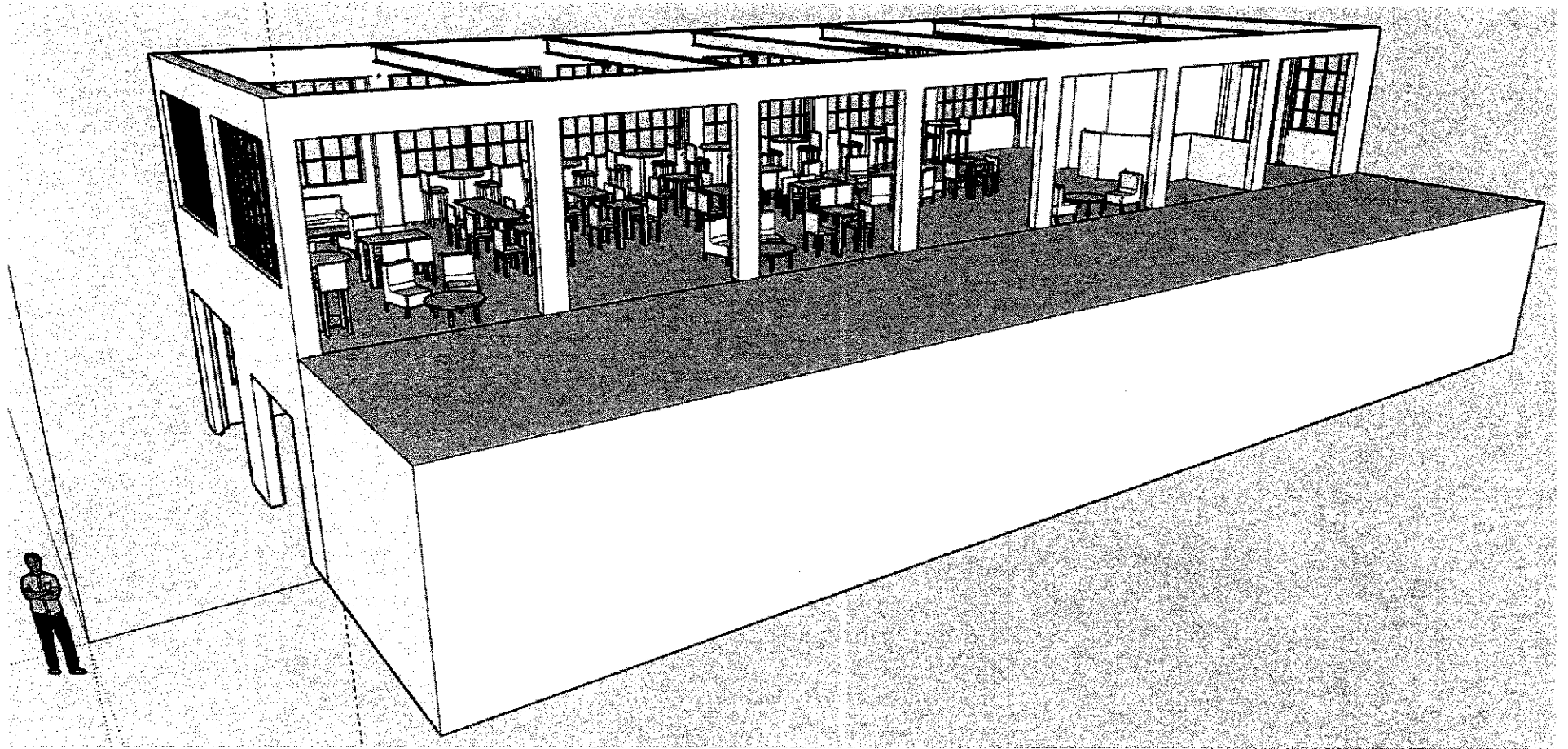
RECEIVED

JUN _ 6 2024

CITY OF PLYMOUTH
COMMUNITY DEVELOPMENT

PUD24-01 230 Wilcox Road
Preliminary PUD Review
PC Mtg 6/12/24

Upper Level Coffee Shop



Upper Level Coffee Shop

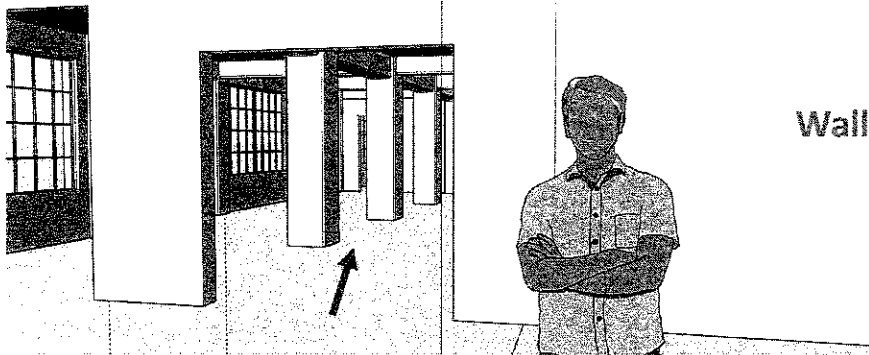
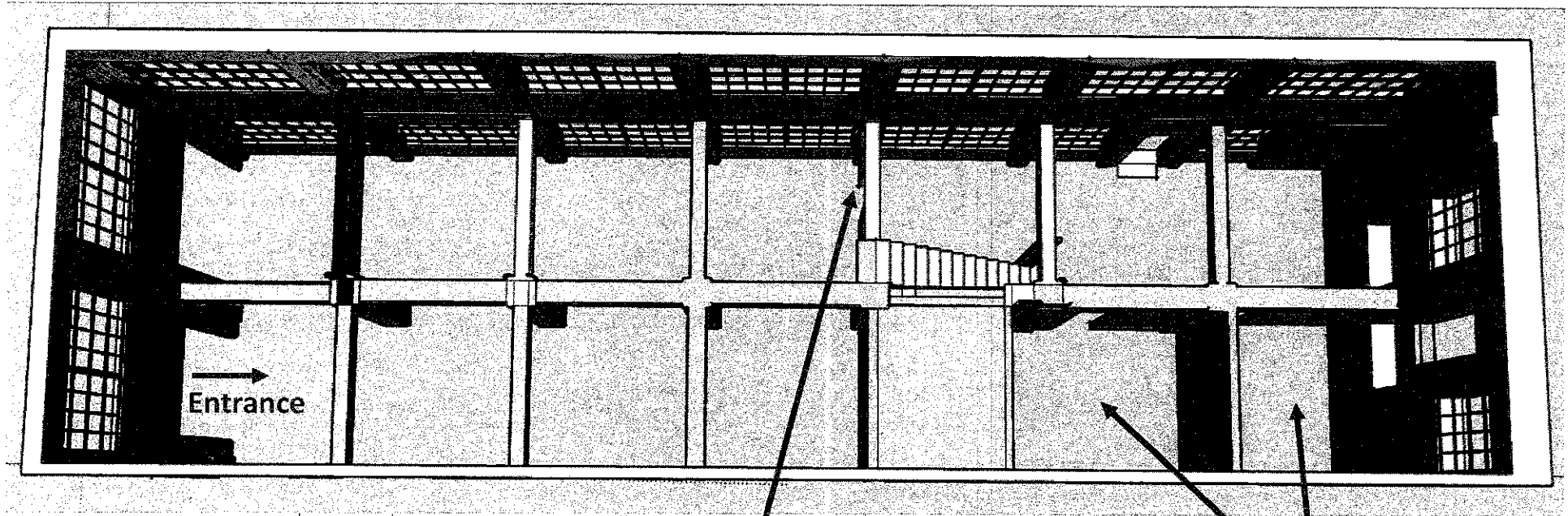


Bathrooms

Service Counter

Entrance

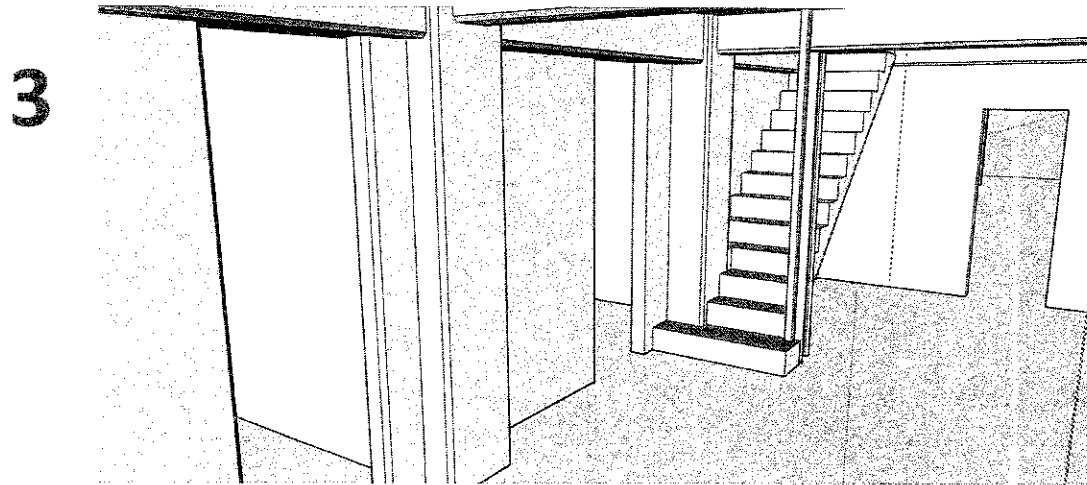
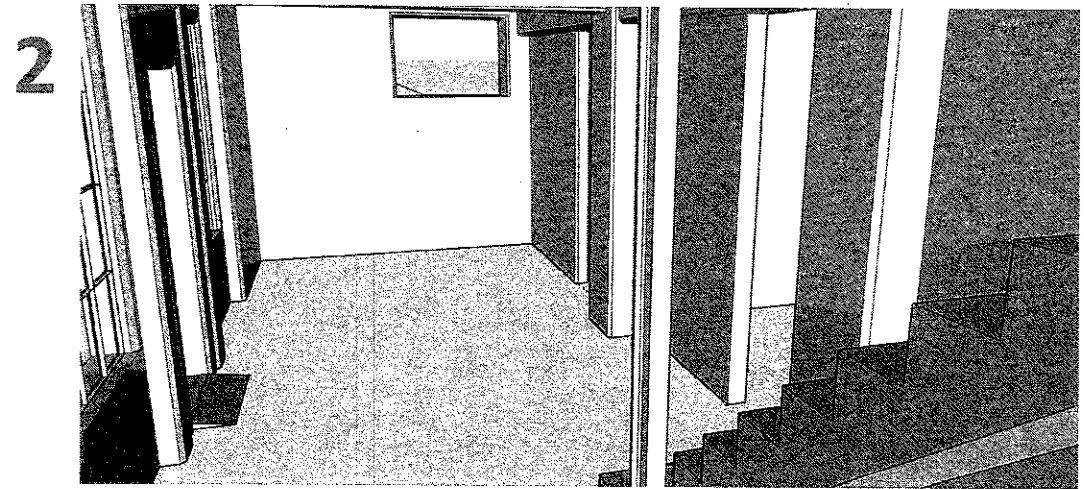
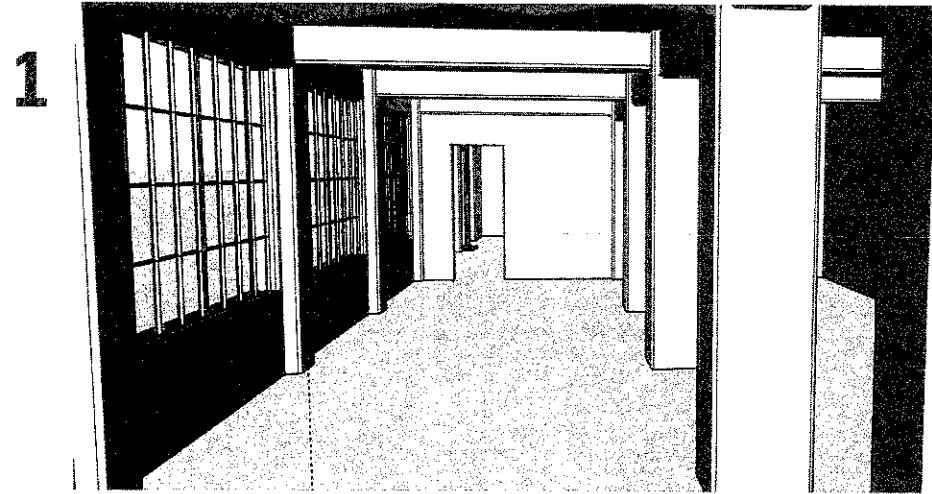
Lower Level – Retail/Roaster/TBD



Wall / Doorway

Bathrooms / Storage

Lower Level – Retail/Roaster/TBD



1 – Walking through lower level

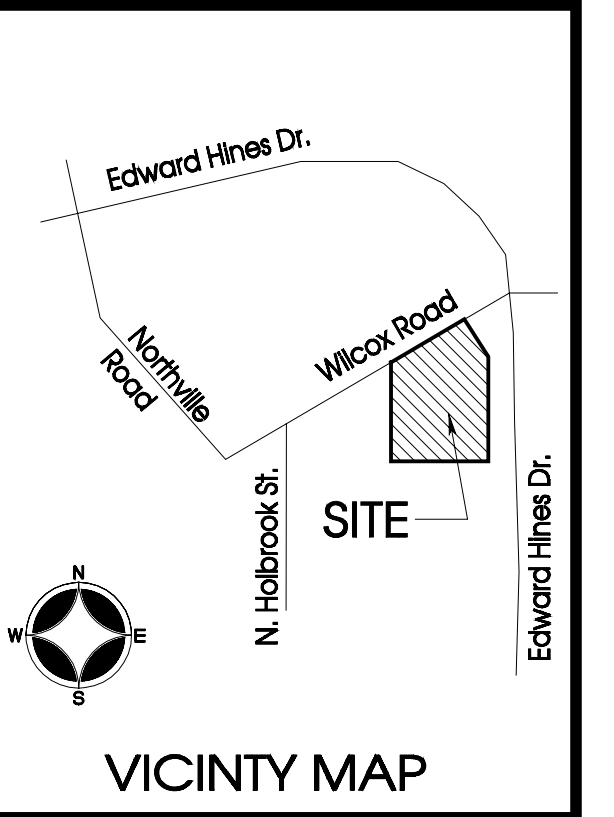
2- Walking past doorway (external door on left, bathrooms/storage and stairs on right)

3- In corner, looking back at doorway

PROPOSED SITE IMPROVEMENT PLANS

DONOFRIO WIL COX, LLC

CITY OF PLYMOUTH, WAYNE COUNTY, MICHIGAN.

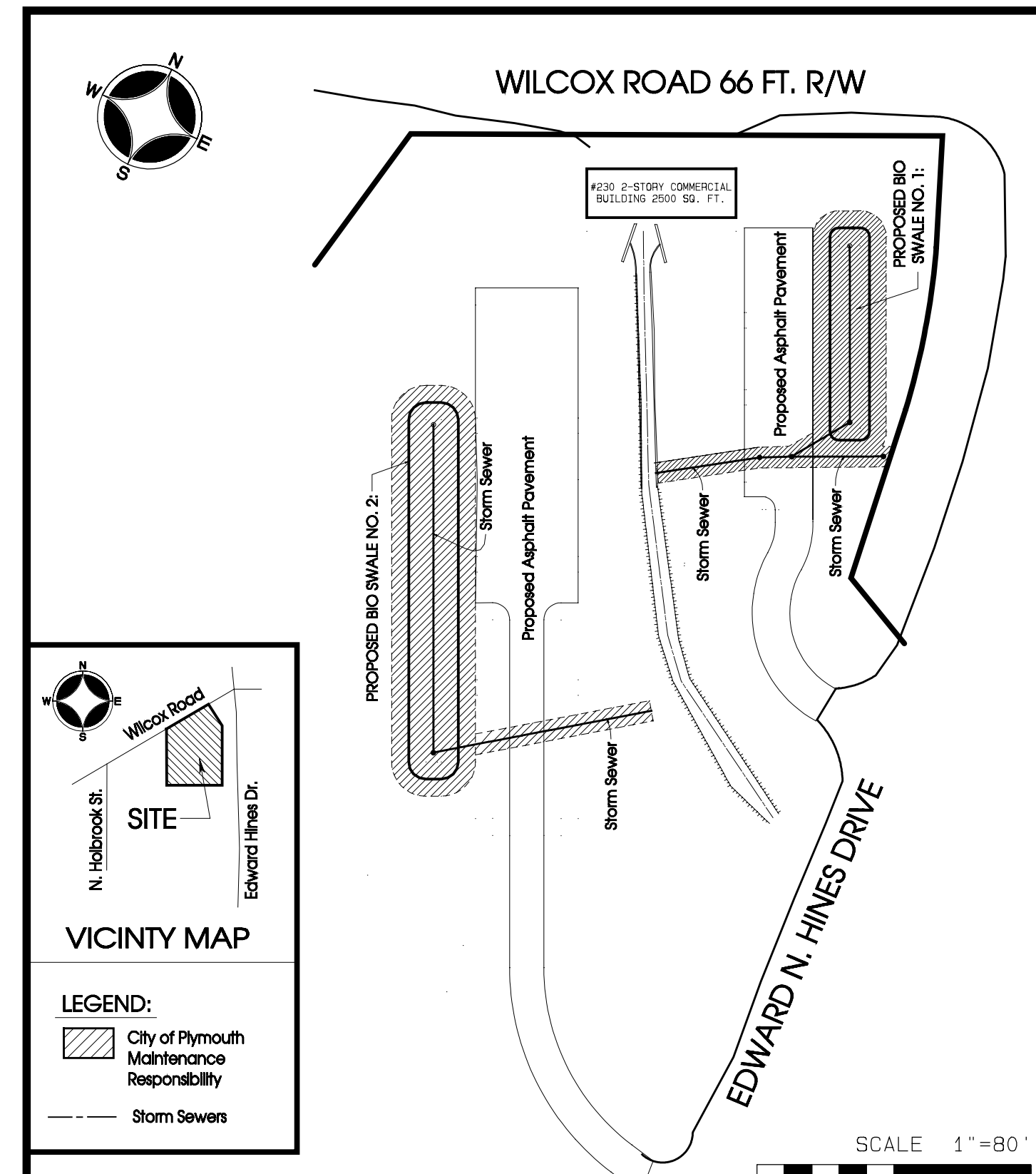


GENERAL NOTES:

- CONTRACTOR IS RESPONSIBLE FOR LOCATING AND VERIFYING ALL EXISTING UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION, AND IS RESPONSIBLE FOR ANY DAMAGE TO THEM DURING CONSTRUCTION.
- LOCATIONS OF EXISTING UNDERGROUND UTILITIES, WATER MAIN, GAS MAIN, ELECTRICAL AND TELEPHONE LINES WERE OBTAINED FROM EXISTING TOPOGRAPHICAL SURVEY (BY OTHERS), NO GUARANTEE CAN BE MADE TO THE COMPLETENESS OR EXACTNESS OF THE LOCATIONS. UNDERGROUND UTILITIES NOT SHOWN HEREIN MAY EXIST.
- ALL DIMENSIONS ARE TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
- CURRENT ZONING: R-1 (PROPERTY WILL REQUIRE RE-ZONING) CONSULT WITH CITY OF PLYMOUTH FOR REZONING APPLICATION AND REQUIREMENTS.
- PARKING CALCULATIONS:
EXISTING BUILDING 3,950 SQ. FT.
ONE FOR EACH 75 SQUARE FEET OF USABLE FLOOR AREA
= 3950/75 = 53
53 PARKING SPACES REQUIRED
54 PARKING SPACES PROVIDED
- GENERAL CONTRACTOR TO PROVIDE AND INSTALL HANDICAP PARKING SIGNS, SIGNS PROVIDED BY CONTRACTOR TO MEET LOCAL REQUIREMENTS.
- PARKING SPACE STRIPING TO BE PAINTED YELLOW (TYPICAL)
- SITE LIGHTS, CONTROLS AND POWER BY GENERAL CONTRACTOR. ASSEMBLY NO. BE-201-DLS-X, FIXTURE TYPE: P16A-SH10
- THE PARKING LOT LIGHTS WILL BE ON A TIMER TO BE COMPLIANT WITH HOURS OF OPERATION.
- A WAYNE COUNTY SOIL EROSION PERMIT WILL BE REQUIRED.

CONSTRUCTION NOTES

- ORGANIC SOILS AND UNSUITABLE MATERIALS SHALL BE STRIPPED FROM AREAS OF PROPOSED PAVEMENT CONSTRUCTION. BACKFILL WITH GRANULAR OR OTHER APPROVED MATERIALS TO MEET AGENCY SPECIFICATIONS.
- ANY RIGHT-OF-WAY OR EASEMENTS NEEDED TO INSTALL THE PROPOSED IMPROVEMENTS MUST BE OBTAINED PRIOR TO CONSTRUCTION.
- PRIOR TO INSTALLATION OF THE ASPHALT LEVELING COURSE OR SECOND LIFT OF ASPHALT BASE, CURB BACKFILL MUST BE INSTALLED AND COMPACTED TO THE TOP OF CURB.
- A PERMIT MUST BE OBTAINED FROM THE WAYNE CO. PERMIT DIVISION PRIOR TO ANY WORK BEING DONE ALONG EXISTING COUNTY ROADS.
- AN EASEMENT FOR ALL PUBLIC STRUCTURES SHALL BE PROVIDED TO CITY OF PLYMOUTH FOR THE PURPOSE OF PROVIDING PUBLIC UTILITIES, INCLUDING WATER, SEWERAGE AND STORM WATER RUNOFF.
- ALL TRENCHES WITHIN A 1 OR 1 SLOPE FROM THE TOP OF CURB SHALL BE BACKFILLED WITH GRANULAR MATERIAL AND COMPACTED TO 95% OF MAXIMUM UNIT WEIGHT.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT CITY REQUIREMENTS.



PROPERTY DESCRIPTION
THE LAND SITUATED IN THE CITY OF PLYMOUTH, COUNTY OF WAYNE, STATE OF MICHIGAN, IS DESCRIBED AS FOLLOWS:
ALL OF LOT 508 AND PART OF LOTS 506 AND 507 ASSESSORS PLYMOUTH PLAT NO 16 AS RECORDED IN L66 P82 OF PLATS WCRCOM AT THE NE COR OF LOT 508 TH FOLLOWING TWO COURSES ALONG THE N LINE OF SAID LOT 506 (1) S 80D 42M 35S W 256.71 FT 2) S 55D 25M 15S W 56.10 FT TO POB; TH CON S 80D 25M 15S W 510.61 FT, TH S 01D 33M 25S E 364.04 FT, TO S LINE OF SAID LOT 508, TH N 88D 25M 05S E 64.50 FT, ALONG SAID LINE; TH N 88D 25M 25S E 240.41 FT; TH S 18D 20M 05S E 28.71 FT; TH S 48D 42M 02S E 127.33 FT; TH N 77D 14M 51S E 50.26 FT; TH 421.09 FT ALONG THE ARC OF AN NON-TANGENT CURVE TO THE LEFT, SAID CURVE HAVING A RADIUS OF 8197.57 FT, A CENTRAL ANGLE OF 03D 53M 35S AND A LONG CHORD BEARING N 14D 48M 45S W 421.01 FT; TH N 19D 47M 11S W 103.60 FT, TH 96.82 FT ALONG THE ARC OF A NON TANGENT CURVE TO THE LEFT, SAID CURVE HAVING A RADIUS OF 456.91 FT, A CENTRAL ANGLE OF 12D 06M 59S AND A LONG CHORD BEARING N 27D 04M 14S W 96.44 FT TH N 34D 42M 56S W 65.15 FT TO A POINT ON THE S LINE OF SAID WILCOX ROAD AND THE POBTS R8E L66 P82 WCR

EXHIBIT "A" ~ PHYSICAL LIMITS OF STORM WATER MANAGEMENT SYSTEM

ALL OF LOT 508 AND PART OF LOTS 506 AND 507, *ASSESSORS PLYMOUTH PLAT NO 16*, CITY OF PLYMOUTH, WAYNE CO., MI.

CLIENT: Donofrio Wil Cox, LLC
1156 Beech St.
Plymouth, MI. 48170
Attn: Greg Donofrio
248.470.4836

Surveying-Engineering-Consultation
TRUE NORTH SURVEYING, INC.
23481 Gloria Drive, Brownstown, MI. 48183
Phone: (734) 676-7799, Fax: (734) 676-4990
Web: TNSurveying.com

SHEET 1 OF 2
Job #: 2024 - 001
Scale: 1" = 80 Feet
Date: 1/08/2024
This Drawing shall not be Reproduced, Modified, Transferred or Issued without the written consent of True North Surveying, Inc. © 2024 True North Surveying, Inc.

STORM WATER MANAGEMENT SYSTEM LONG TERM MAINTENANCE SCHEDULE

Property Information:
DONOFRIO WIL COX, LLC
Contact : Greg Donofrio 248-470-4836

A. PHYSICAL LIMITS OF THE STORM WATER MANAGEMENT SYSTEM
The storm water management system (SWMS) subject to this long term maintenance plan (Plan) is depicted on Exhibit "A" to the permit and inclusive without limitation the storm sewers, swales, manholes, catch basins, storm water inlets and closed conduits that convey the flow from the detention system into the existing storm sewer.

For the purpose of this plan, this storm water management system and all of its components as shown on Exhibit "A" is referred to as Donofrio Wil Cox, LLC SWMS.

B. TIME FRAME FOR LONG TERM MAINTENANCE RESPONSIBILITY
Donofrio Wil Cox, LLC is responsible for maintaining the Donofrio Wil Cox, LLC SWMS, including complying with applicable requirements of the local or Wayne County soil erosion and sedimentation control program. Long term maintenance responsibility for the Donofrio Wil Cox, LLC SWMS commences immediately upon construction approval. Long term maintenance continues in perpetuity.

C. MANNER OF ENSURING MAINTENANCE RESPONSIBILITY
Donofrio Wil Cox, LLC has the responsibility to maintain the system and City of Plymouth retains the right to enter the property and perform the necessary maintenance of the Donofrio Wil Cox, LLC SWMS if the Donofrio Wil Cox, LLC fails to perform the required maintenance activities.

D. LONG TERM MAINTENANCE PLAN AND SCHEDULE
Table 1 identifies the maintenance activities to be performed, organized by category (monitoring/inspection, preventative maintenance and remedial actions) Table 1 also identifies site specific work need to ensure that the storm water management system functions properly.

MAINTENANCE ACTIVITIES	SYSTEM COMPONENT							Frequency
	Storm Sewer System	Inlets	Manholes	Catch Basins	Swales	Storm Water Inlets	Other Areas	
Monitoring/Inspection								
Inspect for sediment accumulation (logging of stone filter)	x	x	x	x	x	x	x	2 times per year
Inspect for floatables, dead vegetation and debris	x	x	x	x	x	x	x	Annually and after major events
Inspect for erosion and integrity of banks and beams	x	x	x	x	x	x	x	Annually and after major events
Inspect all components during wet weather and compare to as-built plans	x	x	x	x	x	x	x	Annually
Monitor plantings/vegetation								2 times per year
Ensure means of access for maintenance remain clear/open	x	x	x	x	x	x	x	Annually
Preventative Maintenance								
Mowing								Up to 2 times/year, select areas only
Remove accumulated sediment	x	x	x	x	x	x	x	As needed
Remove floatables, dead vegetation and debris	x	x	x	x	x	x	x	As needed
Replace subsurface components								Every 3 years or as needed
Remove invasive plant species								Annually
Reapply / Replace mulch layer								Re apply every 6 months, replace every 2 yr.
Sweep paved areas								As needed
Remedial Actions								
Repair/stabilize areas of erosion								As needed
Replace dead plantings, bushes, trees								As needed
Reseed bare areas								As needed
Structural repairs	x	x	x	x	x	x	x	As needed
Make adjustments/repairs to ensure proper functioning	x	x	x	x	x	x	x	As needed

MAINTENANCE
Chemicals shall not be applied to forbay, detention pond, 25 ft. buffer strips and vegetated swales.
Inlets and outlets should be inspected periodically for blockage, signs of soil erosion, and structural damage. Swales should be inspected for sediment accumulation semi-annually and after rain events. Sediment that is impeding flow should be removed. Inlets, outlets, and appurtenances (e.g., grates) should be inspected annually for structural integrity. Outlets should be checked regularly for clogging and should be cleaned when necessary, especially after large storm events. At a minimum when sediments accumulate to a depth of 8"-12" or if sediment resuspension is observed.

EXHIBIT "B" ~ LONG TERM MAINTENANCE PLAN

ALL OF LOT 508 AND PART OF LOTS 506 AND 507, *ASSESSORS PLYMOUTH PLAT NO 16*, CITY OF PLYMOUTH, WAYNE CO., MI.

CLIENT: Donofrio Wil Cox, LLC
1156 Beech St.
Plymouth, MI. 48170
Attn: Greg Donofrio
248.470.4836

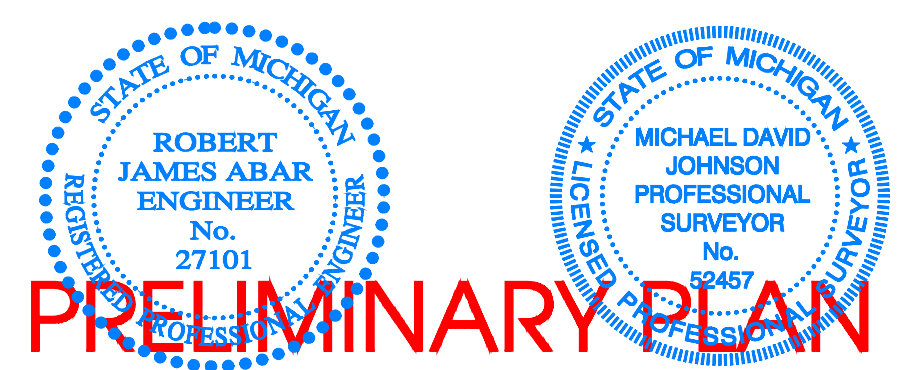
Surveying-Engineering-Consultation
TRUE NORTH SURVEYING, INC.
23481 Gloria Drive, Brownstown, MI. 48183
Phone: (734) 676-7799, Fax: (734) 676-4990
Web: TNSurveying.com

SHEET 2 OF 2
Job #: 2024 - 001
Scale: 1" = N/A Feet
Date: 1/08/2024
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SHEET INDEX: PG.

- Cover Sheet 1
- Topographical Survey (by others) 2
- Site Plan 3
- Storm Sewer Profiles and Details 4
- Soil Erosion & Sedimentation Control Plan 5
- Landscape Plan (by others) 6
- Standard Details 7 ~ 13
ME1, SE1, ST1, ST2, SW1, SS1, SS2

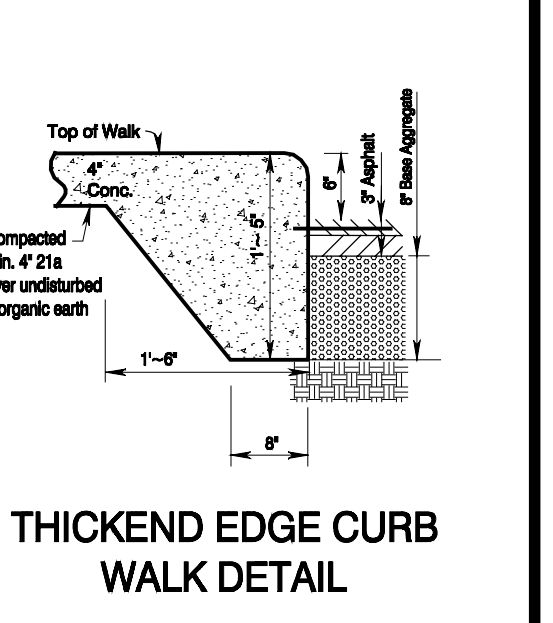
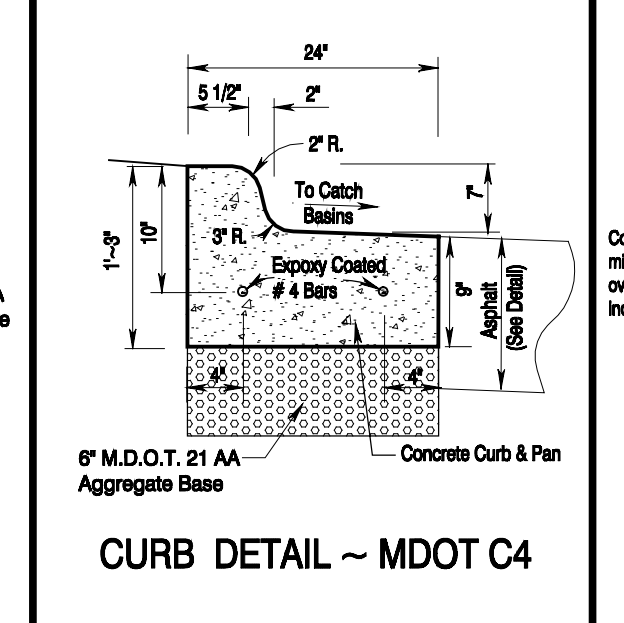
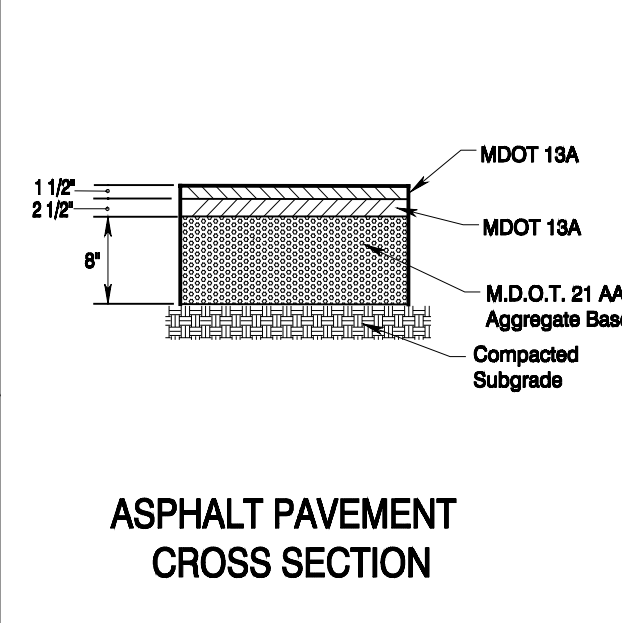
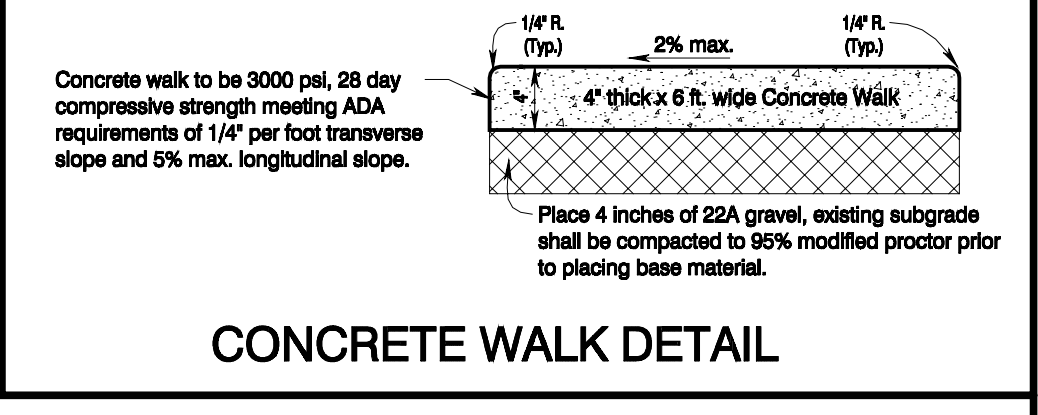
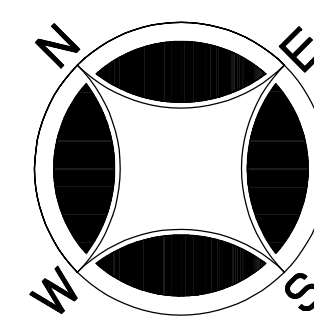
PROPERTY DESCRIPTION
THE LAND SITUATED IN THE CITY OF PLYMOUTH, COUNTY OF WAYNE, STATE OF MICHIGAN, IS DESCRIBED AS FOLLOWS:
AUL OF LOT 508 AND PART OF LOTS 506 AND 507 ASSESSORS PLYMOUTH PLAT NO 16 AS RECORDED IN L66 P82 OF PLATS WCRCOM AT THE NE COR OF LOT 506 TH FOLLOWING TWO COURSES ALONG THE N LINE OF SAID LOT 506 (1) S 80D 42M 35S W 256.71 FT 2) S 55D 25M 15S W 56.10 FT TO POB; TH CON S 80D 25M 15S W 510.61 FT, TH S 01D 33M 25S E 364.04 FT, TO S LINE OF SAID LOT 508, TH N 88D 25M 05S E 64.50 FT, ALONG SAID LINE; TH N 88D 25M 25S E 240.41 FT; TH S 18D 20M 05S E 28.71 FT; TH S 49D 42M 02S E 127.33 FT; TH N 77D 14M 51S E 50.26 FT; TH 421.09 FT ALONG THE ARC OF AN NON-TANGENT CURVE TO THE LEFT, SAID CURVE HAVING A RADIUS OF 8197.57 FT, A CENTRAL ANGLE OF 03D 53M 35S AND A LONG CHORD BEARING N 14D 48M 45S W 421.01 FT; TH N 19D 47M 11S W 103.60 FT, TH 96.82 FT ALONG THE ARC OF A NON TANGENT CURVE TO THE LEFT, SAID CURVE HAVING A RADIUS OF 456.91 FT, A CENTRAL ANGLE OF 12D 06M 59S AND A LONG CHORD BEARING N 27D 04M 14S W 96.44 FT TH N 34D 42M 56S W 65.15 FT TO A POINT ON THE S LINE OF SAID WILCOX ROAD AND THE POBTS R8E L66 P82 WCR



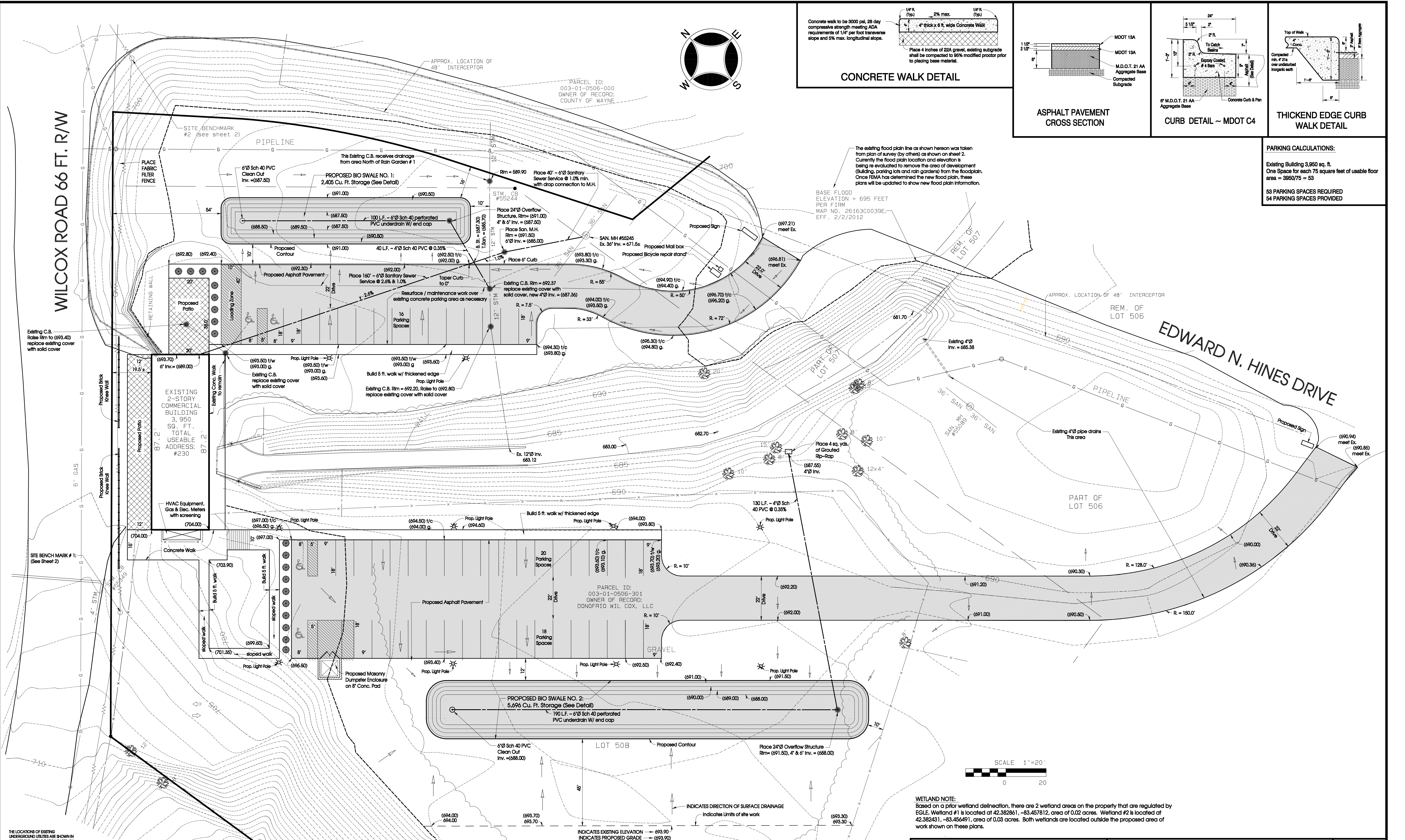
Robert James Abor, P.E. # 27101
Michael D. Johnson, P.S. # 52457

DEVELOPER: Wil Cox, LLC 1156 Beech St. Plymouth, MI. 48170 Attn: Greg Donofrio 248.470.4836	COVER SHEET FOR: DONOFRIO WIL COX, LLC	TRUE NORTH SURVEYING, INC. 23481 Gloria Drive, Brownstown, MI. 48183 Phone: (734) 676-7799, Fax: (734) 676-4990 Web: TNSurveying.com Scale: 1" = N/A Feet Date: 1/08/2024	REVISIONS: SHEET 1 OF 13 Job #: 2024 - 001
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WILCOX ROAD 66 FT. R/W



PARKING CALCULATIONS:
Existing Building 3,950 sq. ft.
One Space for each 75 square feet of usable floor area = 3950/75 = 53
53 PARKING SPACES REQUIRED
54 PARKING SPACES PROVIDED



The existing flood plain line as shown hereon was taken from plan of survey (by others) as shown on sheet 2. Currently the flood plain location and elevation is being re-evaluated to remove the area of development (Building, parking lots and rain gardens) from the floodplain. Once FEMA has determined the new flood plain, these plans will be updated to show new flood plain information.

BASE FLOOD ELEVATION = 695 FEET PER FIRM MAP NO. 26163C0039E, EFF. 2/2/2012

WETLAND NOTE:
Based on a prior wetland delineation, there are 2 wetland areas on the property that are regulated by EGLE. Wetland #1 is located at 42.382861, -83.457812, area of 0.02 acres. Wetland #2 is located at 42.382431, -83.456491, area of 0.03 acres. Both wetlands are located outside the proposed area of work shown on these plans.

SITE DEVELOPMENT PLAN FOR: DONOFRIO WIL COX, LLC

ALL OF LOT 508 AND PART OF LOTS 506 AND 507, 'ASSESSORS PLYMOUTH PLAT NO 18', CITY OF PLYMOUTH, COUNTY OF WAYNE, STATE OF MICHIGAN, AS RECORDED IN L. 66, P. 82 OF PLATS W.C.R.

TRUE NORTH SURVEYING, INC. 23481 Glorita Drive, Brownstown, MI 48183 Phone: (734) 676-7799, Fax: (734) 676-4990 Web: TNsurveying.com

Scale: 1" = 20 Feet Date: 1/08/2024

Table with 2 columns: REVISIONS, SHEET 3 OF 13, Job #: 2024-001

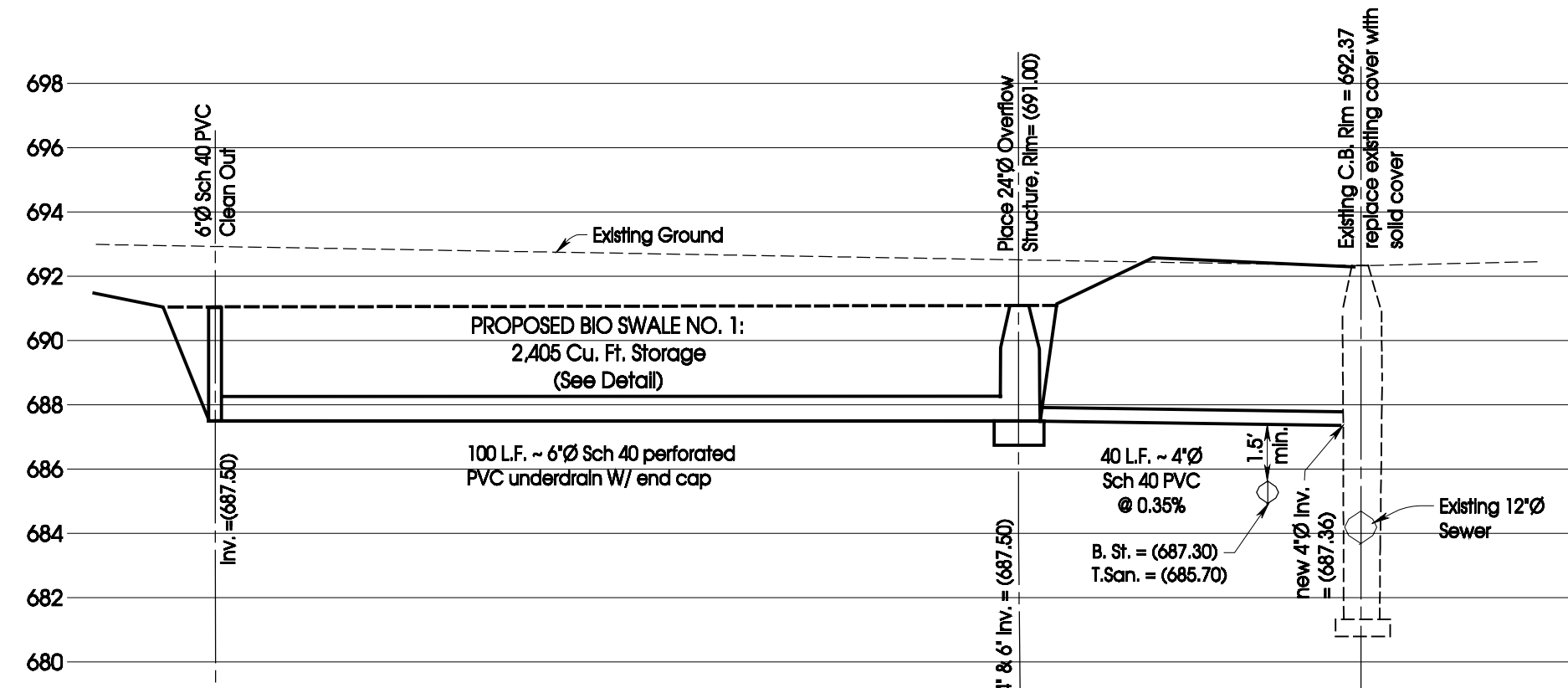
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE LOCATION BASED ON BEST AVAILABLE INFORMATION ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR TRUE NORTH SURVEYING, INC. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCURRED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.



LEGEND:
950.00 - Indicates existing elevation (950.00) - Indicates proposed grades

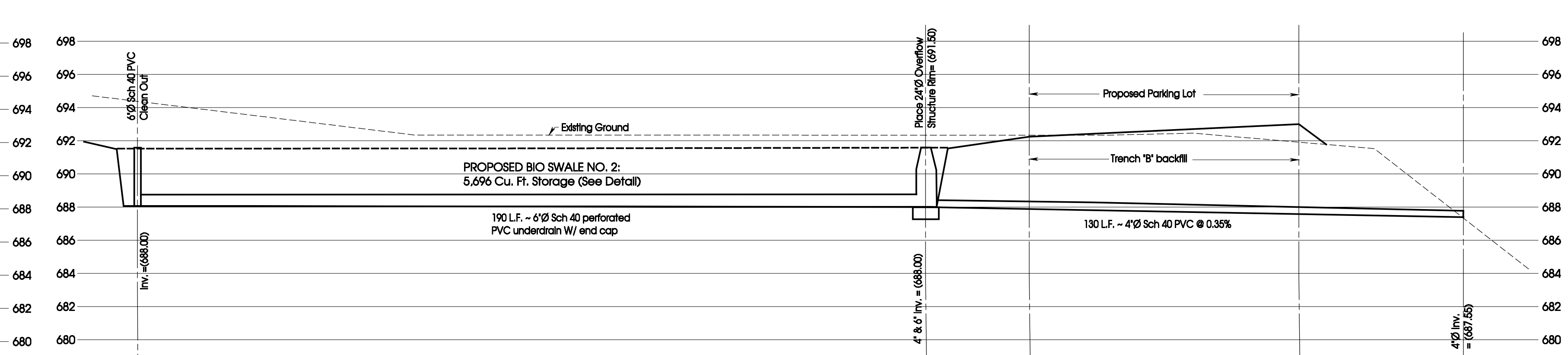
INDICATES EXISTING ELEVATION - 950.00
INDICATES PROPOSED GRADE - (950.00)

NOTICE:
THE DEVELOPER IS RESPONSIBLE FOR RESOLVING ANY DRAINAGE PROBLEMS ON ADJACENT PROPERTIES WHICH ARE A RESULT OF THE DEVELOPER'S ACTIVITIES.



**STORM SEWER PROFILE
BIO SWALE # 1**

Scale: Horizontal: 1" = 20', Vertical: 1" = 5'



**STORM SEWER PROFILE
BIO SWALE # 2**

Scale: Horizontal: 1" = 20', Vertical: 1" = 5'

STORM SEWER RUNOFF VOLUME CALCULATIONS:

Bio Swale # 1 (NORTH SIDE OF SITE)

Area draining into the proposed Bio Swale = 0.377 acres

Land Use	Area	Runoff Coefficient
Impervious	0.361 ac	0.95
Lawn	0.016 ac	0.30

DRAINAGE DISTRICT:

Area #1 - Into Bio Swale - 0.377 acres
Existing Lawn area north of proposed Bio Swale
No. 1 drains in an existing Catch basin located near
the S.E. corner of the proposed Bio Swale # 1

Closed Conduit Sizing - Rational Method
Q = C I A

Weighted Runoff Coefficient:
C = (0.361 x 0.95) + (0.016 x 0.30) / 0.377 = 0.92

Use 10 min. for initial time of concentration

Intensity for 100 year storm
I = 101 / (12.33 + T) ^ 0.84 = 7.4 in/hr

Vcpvc = A * C * 3,630
Vcpvc = 0.377 x 0.92 x 3630 = 1259 cubic feet

Channel Protection Rate Control (CPRC)

Vcpvc = A * C * 6,897
Vcpvc = 0.377 x 0.92 x 6897 = 2,392 cubic feet

100-year Flood Control Volume Calculations

100-yr Allowable Release Rate Q allowable
Q allow = 1.1055 - 0.207 ln (A)
Q allow = 1.1055 - 0.207 ln (0.377) = 1.307 cfs/acre

A site that is less than 2 acres must use variable release rate of 1 cfs/acre.

100 yr Peak allowable Discharge (Qo)
Qo = Q allow x A

Qo = 1 x 0.377 = 0.377 cfs

Calculate 100-year Pond Volume:

100-yr Peak Pond Inflow Qi = C x I100 x A
Qi = 0.92 x 7.4 x 0.377 = 2.567 cfs

100-yr Runoff Volume (Vr):

Vr = 18,900 x C x A
Vr = 18,900 x 0.92 x 0.377 = 6,555

Storage Ratio (Vr/Vs):

Vs / Vr = 0.206 - 0.15 ln (Qo / Qi)
Vs/Vr = 0.206 - 0.15 ln (0.377/2.567) = 0.4937

100-yr Required Storage Volume (Vs):

Vs = Vr x Storage Ratio or Vs = Vr x (0.206 - 0.15 ln (Qo / Qi))
Vs = 6,555 x 0.4937 = 3,236

The site plan must accommodate the following volumes:

CPVC: 1,259 cu. ft.
CPRC: 2,392 cu. ft.
Flood Control: 3,236 - 1,259 = 1,977

Orifice Calculations Outlet Restrictor Size for CPRC (Extended Detention)

Average Discharge Rate (Qave):

Qave = CPRC Vol / 172,800 = cfs

Qave = 1259/172800 = 0.00729

Bio Swale storage depth = 3.5 ft.

Have = 3.5/2 = 1.75 ft.

Extended Detention Office Area (a):

A = qave / 0.62 sqrt 2 x G x have

0.00729 / 0.62 x sq rt 64.4 x 1.75 = 0.00756/7.87 = 0.0011076 sq. ft.

Extended Detention Office Diameter (d):

d = 12 x sqrt 4 x (A/ pie) = 12 x sqrt 4 x (0.0011076/ pie) = 0.45 Dia inches

The calculated orifice size, 0.45 inches, is less than the minimum orifice size of 1 inch. Set the orifice size at 1 inch; this automatically meets the Extended Detention requirement, use 4" dia. To prevent clogging.

STORAGE PROVIDED IN BIO SWALE NO. 1:

Storage in Surface Pond	Elevation	Area	Volume
(max. 6" deep)...	691.00	2830.	
	690.50	2548	(2830 + 2548) / 2 x 0.5 = 1345 cu. ft.

Storage in Planting Soil And Gravel Layers (Based on 20% void)	Elevation	Area	Volume
	690.50	2548	
	687.50	990	(2548 + 990) / 2 x 3 x 20% = 1061 cu. ft.

Total Volume Required = 1,977 cu. ft.
Total Volume Provided = 1345 + 1061 = 2,405 cu. ft.

BIO SWALE # 2 (SOUTH SIDE OF SITE)

Area draining into the proposed Bio Swale = 1.45 acres

Land Use	Area	Runoff Coefficient
Impervious	0.998 ac	0.95
Lawn	0.452 ac	0.30

DRAINAGE DISTRICTS:

Area #2 - Into Bio Swale - 1.45 acres
Existing Lawn area north of drive drains
into existing 4" dia. Pipe.

Closed Conduit Sizing - Rational Method
Q = C I A

Weighted Runoff Coefficient:
C = (0.998 x 0.95) + (0.452 x 0.30) / 1.45 = 0.75

Use 10 min. for initial time of concentration

Intensity for 100 year storm
I = 101 / (12.33 + T) ^ 0.84 = 7.4 in/hr

Vcpvc = A * C * 3,630
Vcpvc = 1.45 x 0.75 x 3630 = 3947.63 cubic feet

Channel Protection Rate Control (CPRC)

Vcpvc = A * C * 6,897
Vcpvc = 1.45 x 0.75 x 6897 = 7500 cubic feet

100-year Flood Control Volume Calculations

100-yr Allowable Release Rate Q allowable
Q allow = 1.1055 - 0.207 ln (A)
Q allow = 1.1055 - 0.207 ln(1.45) = 1.029 cfs/acre

A site that is less than 2 acres must use variable release rate of 1 cfs/acre.

100 yr Peak allowable Discharge (Qo)
Qo = Q allow x A

Qo = 1 x 1.45 = 1.45 cfs

Calculate 100-year Pond Volume:

100-yr Peak Pond Inflow Qi = C x I100 x A
Qi = 0.75 x 7.4 x 1.45 = 8.04 cfs

100-yr Runoff Volume (Vr):

Vr = 18,900 x C x A
Vr = 18,900 x 0.75 x 1.45 = 20,554

Storage Ratio (Vr/Vs):

Vs / Vr = 0.206 - 0.15 ln (Qo / Qi)
Vs/Vr = 0.206 - 0.15 ln (1.45/8.04) = 0.463

100-yr Required Storage Volume (Vs):

Vs = Vr x Storage Ratio or Vs = Vr x (0.206 - 0.15 ln (Qo / Qi))
Vs = 20,554 x 0.463 = 9517

The site plan must accommodate the following volumes:

CPVC: 3,948 cu. ft.
CPRC: 7,500 cu. ft.
Flood Control: 9517 - 3948 = 5569

Orifice Calculations Outlet Restrictor Size for CPRC (Extended Detention)

Average Discharge Rate (Qave):

Qave = CPRC Vol / 172,800 = cfs

Qave = 3948/172800 = 0.02284

Bio Swale storage depth = 3.5 ft.

Have = 3.5/2 = 1.75 ft.

Extended Detention Office Area (a):

A = qave / 0.62 sqrt 2 x G x have

0.02284 / 0.62 x sq rt 64.4 x 1.75 = 0.00756/7.87 = 0.0347 sq. ft.

Extended Detention Office Diameter (d):

d = 12 x sqrt 4 x (A/ pie) = 12 x sqrt 4 x (0.0347/ pie) = 2.52 Dia inches

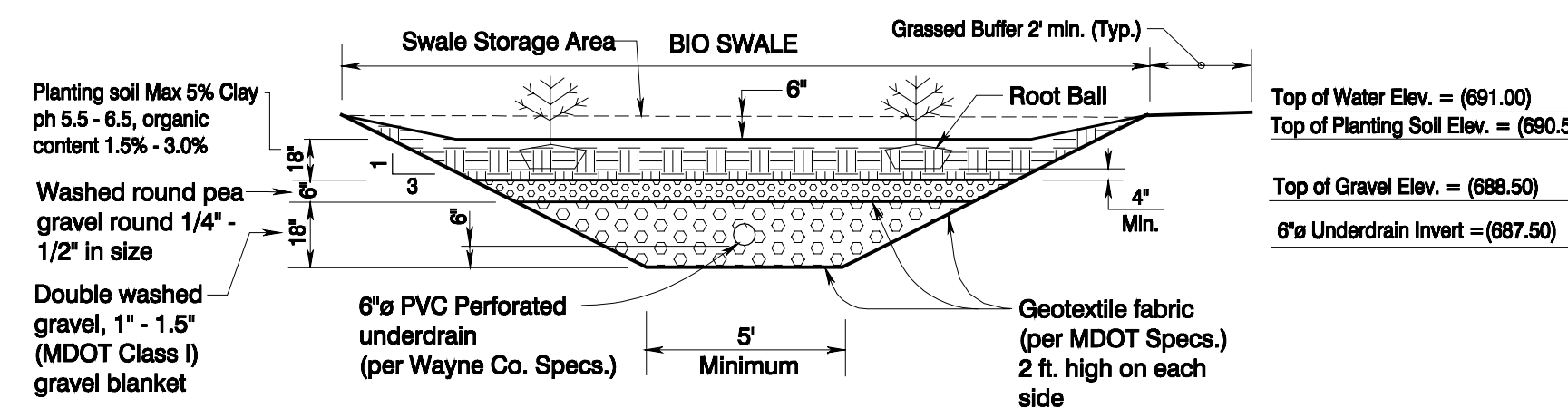
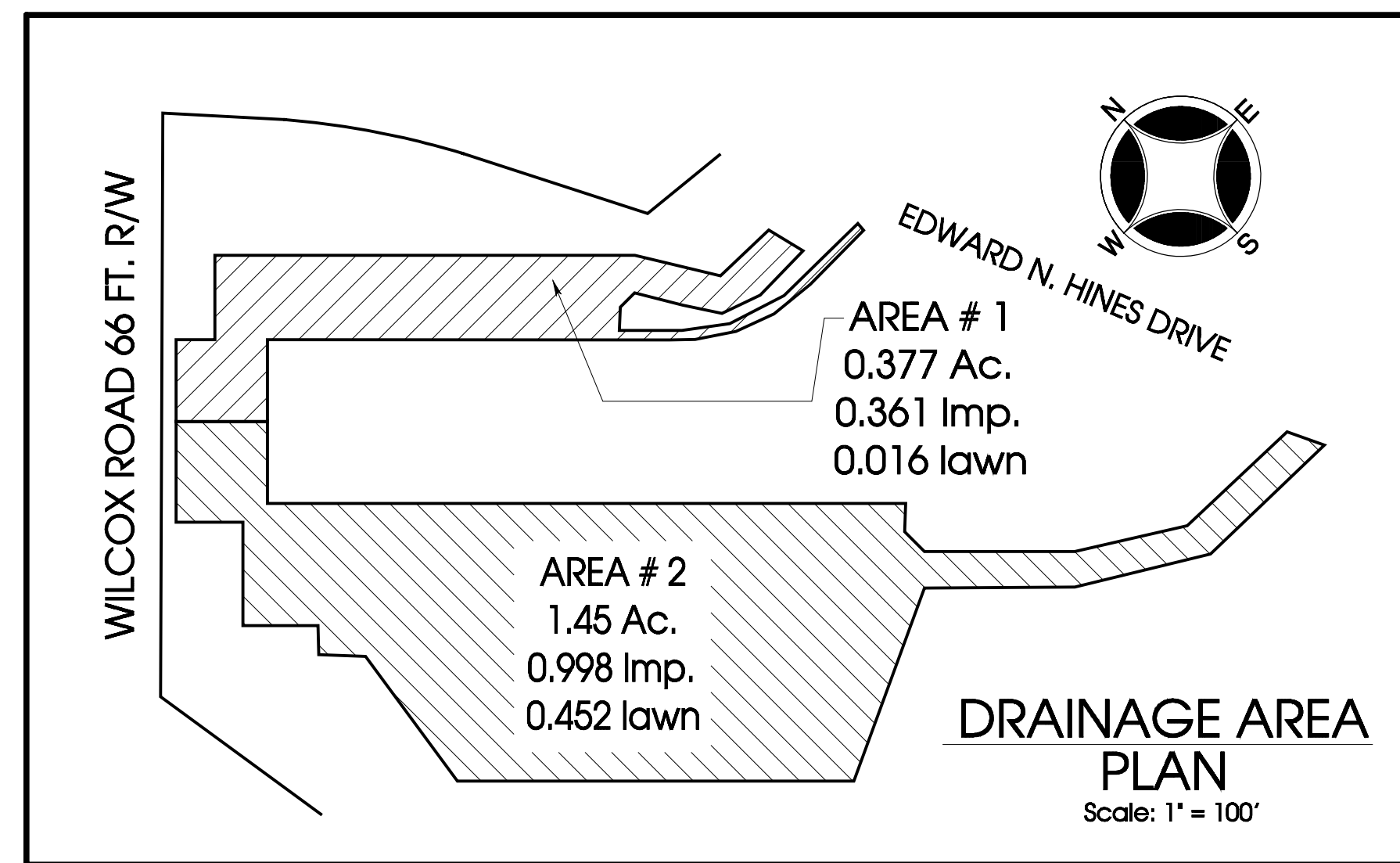
The calculated orifice size, 2.52 inches, use 4" dia/ to prevent clogging.

STORAGE PROVIDED IN BIO SWALE NO. 1:

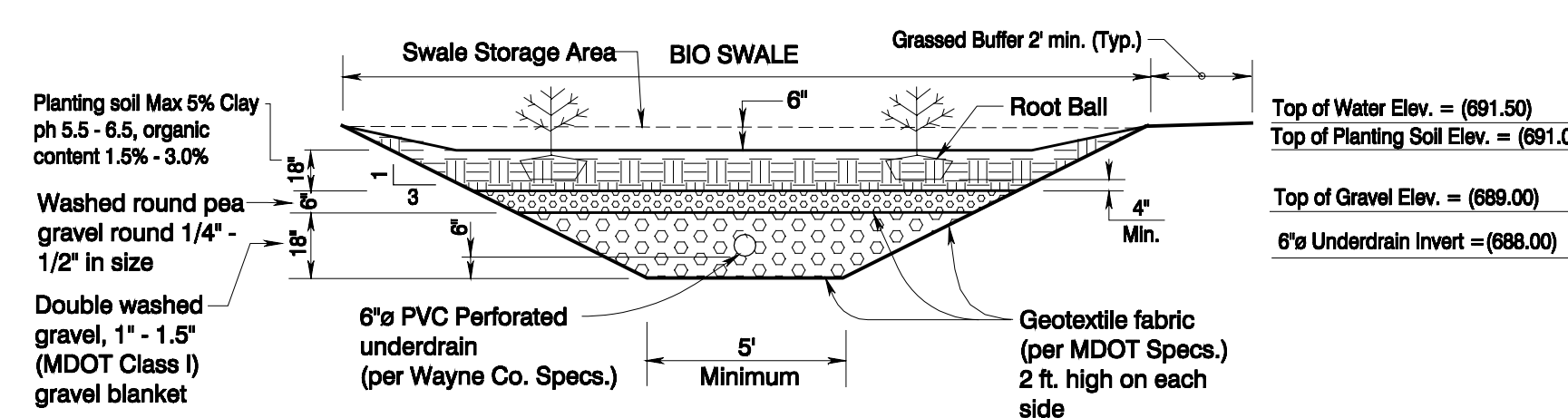
Storage in Surface Pond	Elevation	Area	Volume
(max. 6" deep)...	691.50	6293.	
	691.00	5816	(6293 + 5816) / 2 x 0.5 = 3027 cu. ft.

Storage in Planting Soil And Gravel Layers (Based on 20% void)	Elevation	Area	Volume
	691.00	5816	
	688.00	3081	(5816 + 3081) / 2 x 3 x 20% = 2669 cu. ft.

Total Volume Required = 5569 cu. ft.
Total Volume Provided = 3027 + 2669 = 5696 cu. ft.



BIO SWALE #1: CROSS SECTION



BIO SWALE #2: CROSS SECTION

BIO SWALE RECOMMENDED PLANTING LIST:

- RECOMMENDED SHRUBS**
- American Cranberrybush - Viburnum trilobum
 - Black Chokeberry - Aronia prunifolia
 - Buttonbush - Cephalanthus occidentalis
 - Fragrant Sumac - Rhus aromatica
 - Meadowweet - Spirea alba
 - Nannyberry Viburnum - Viburnum lentago
 - Ninebark - Physocarpus opulifolius
 - Redswire Dogwood - Cornus stolonifera
 - Shrubby Cinqufoil - Potentilla fruticosa
 - Shrub St. John's-Wort - Hypericum prostratum
 - Spicebush - Lindera benzoin
 - Steeplebush - Spirea tomentosa
 - Virginia Sweetpirra - Ilex virginica
- RECOMMENDED WILDFLOWERS**
- Beechdrops - Pteris aquilina
 - Bergamot (Bee-Balm) - Monarda fistulosa
 - Black-Eyed Susan - Rudbeckia hirta
 - Blue Flag Iris - Iris versicolor
 - Blue Vervain - Veronica hastata
 - Boneset - Eupatorium perfoliatum
 - Cardinal Flower - Lobelia cardinalis
 - Columbine - Aquilegia canadensis
 - Culver's Root - Veronastrium virginicum
 - Golden Alexander - Zizia aurea
 - Great Blue Lobelia - Lobelia siphilitica
 - Heath Aster - Aster ericoides
 - Ironweed - Vernonia missouriica
- Job-Pye Weed - Eupatorium fistulosum**
- Marsh Bleeding Star - Liatris spicata**
- Mountain-Mint - Pycnanthemum virginicum**
- New England Aster - Aster novae-angliae**
- Queen-of-the-Prairie - Filipendula rubra**
- Sneezeweed - Helenium autumnale**
- Spikewort - Tradescantia ohiensis**
- Sweet Pickens - Solidago patula**
- Three-Loed Coneflower - Rudbeckia triloba**
- White Turtlehead - Chelone glabra**
- Wild Strawberry - Fragaria virginiana**



DONOFRIO WIL COX, LLC
STORM SEWER PROFILES AND DETAILS

ALL OF LOT 508 AND PART OF LOTS 506 AND 507,
"ASSESSORS PLYMOUTH PLAT NO 16", CITY OF
PLYMOUTH, COUNTY OF WAYNE, STATE OF MICHIGAN,
AS RECORDED IN L. 66, P. 82 OF PLATS W.C.R.

Surveying-Engineering-Consultation
TRUE NORTH SURVEYING, INC.
23481 Gloria Drive, Brownstown, MI 48183
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Scale: As Shown Date: 1/08/2024

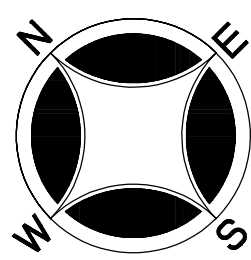
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NO.	DATE	DESCRIPTION

SHEET 4 OF 13
Job #: 2024 - 001

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WILCOX ROAD 66 FT. R/W



SOIL EROSION & SEDIMENTATION CONTROL NOTES:

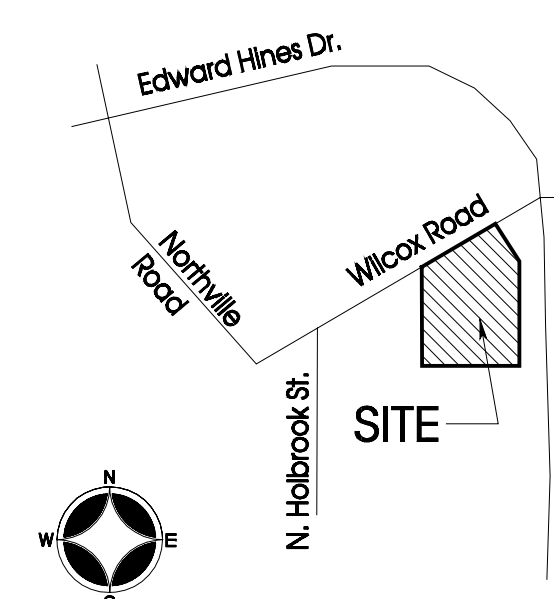
- Based on the Wayne County Soil Survey the subject site contains Cohoctah fine sandy loam
- The proposed earth change area = 1.74 Acres
- All items on approved SESC plan and permit must be completed.
- All disturbed earth must be permanently stabilized with vegetation or hard surface.
- All temporary SESC devices may be removed after Certificate of Completion.
- Accumulated sediment must be removed from the entire storm sewer system.
- All stockpiled materials/topsoil shall be protected with silt fence until permanently stabilized. Material shall be used on site for final grading and landscaping.

SESC MAINTENANCE PLAN

- Contractor shall be responsible for all SESC maintenance including all of the following (as a minimum):
- Soil tracked, spilled, dumped or deposited onto public streets, highways, sidewalks or other public thoroughfares must be removed promptly. Street scraping shall be performed daily along with weekly street sweeping (as a minimum).
- A temporary crushed rock tracking pad must be installed at the construction entrance and exit. This tracking pad must be maintained with fresh stone periodically. Construction must be limited to designated entrance and exit.
- Daily Dust control.
- Inspect SESC devices on a weekly basis or within 24 hours of every rain event.

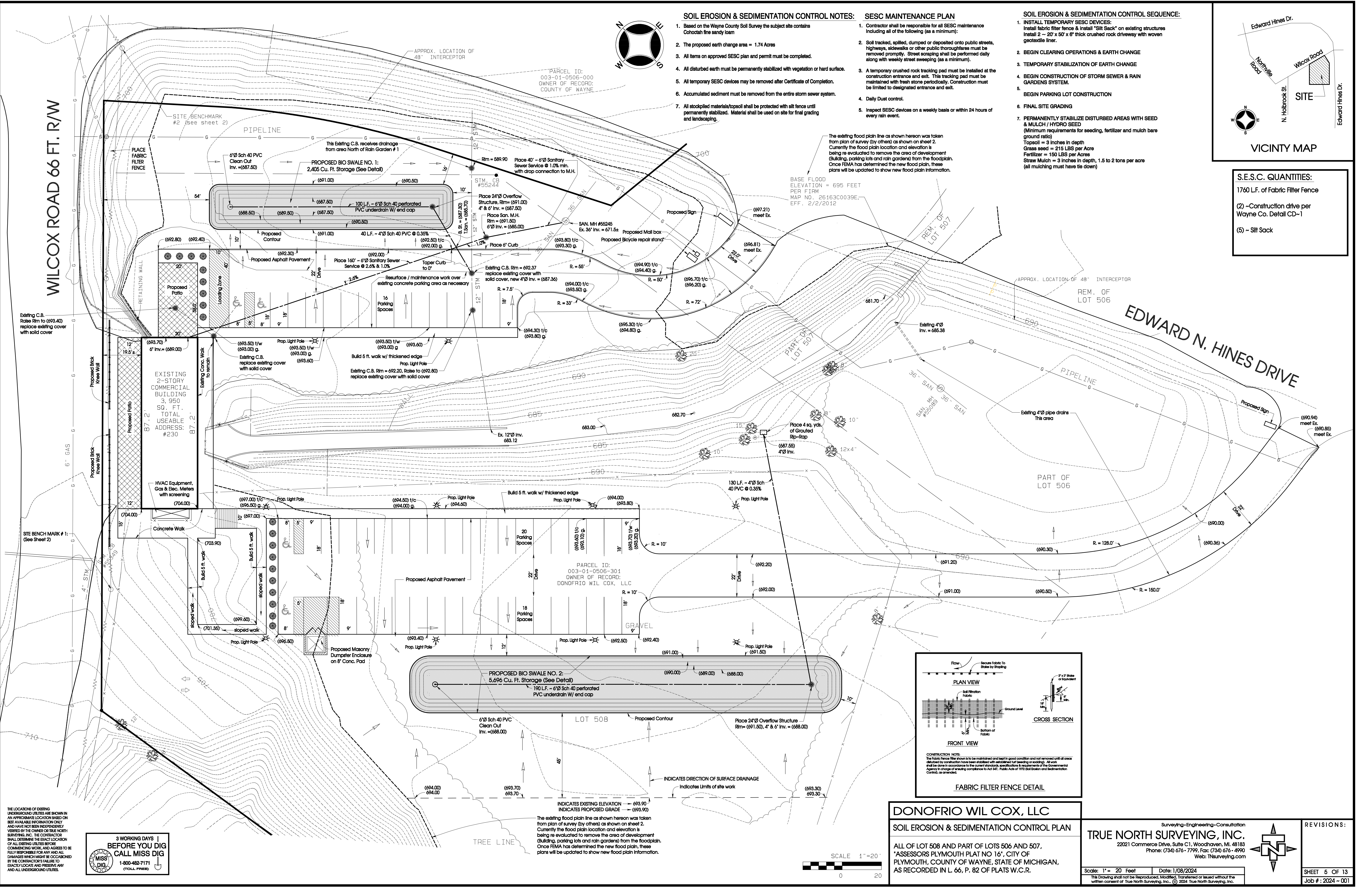
SOIL EROSION & SEDIMENTATION CONTROL SEQUENCE:

- INSTALL TEMPORARY SESC DEVICES:
Install fabric filter fence & install "Silt Sack" on existing structures Install 2' x 20' x 6" thick crushed rock driveway with woven geotextile liner.
- BEGIN CLEARING OPERATIONS & EARTH CHANGE
- TEMPORARY STABILIZATION OF EARTH CHANGE
- BEGIN CONSTRUCTION OF STORM SEWER & RAIN GARDENS SYSTEM.
- BEGIN PARKING LOT CONSTRUCTION
- FINAL SITE GRADING
- PERMANENTLY STABILIZE DISTURBED AREAS WITH SEED & MULCH / HYDRO SEED
(Minimum requirements for seeding, fertilizer and mulch bare ground ratio)
Topsoil = 3 inches in depth
Grass seed = 215 LBS per Acre
Fertilizer = 150 LBS per Acre
Straw Mulch = 3 inches in depth, 1.5 to 2 tons per acre (all mulching must have tie down)



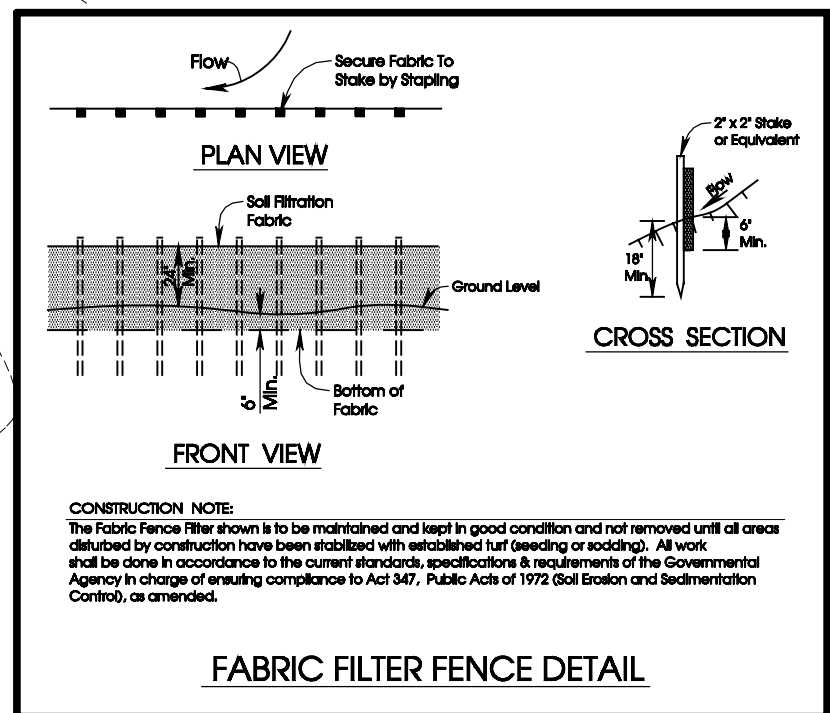
VICINITY MAP

S.E.S.C. QUANTITIES:
 1760 LF. of Fabric Filter Fence
 (2) - Construction drive per Wayne Co. Detail CD-1
 (5) - Silt Sack



The existing flood plain line as shown hereon was taken from plan of survey (by others) as shown on sheet 2. Currently the flood plain location and elevation is being re-evaluated to remove the area of development (Building, parking lots and rain gardens) from the floodplain. Once FEMA has determined the new flood plain, these plans will be updated to show new flood plain information.

BASE FLOOD ELEVATION = 695 FEET PER FIRM MAP NO. 26163C0039E, EFF. 2/2/2012



DONOFRIO WIL COX, LLC

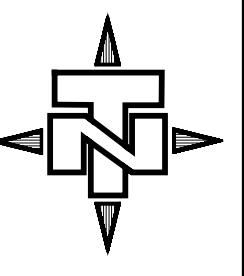
SOIL EROSION & SEDIMENTATION CONTROL PLAN

ALL OF LOT 508 AND PART OF LOTS 506 AND 507,
 'ASSESSORS PLYMOUTH PLAT NO 16', CITY OF
 PLYMOUTH, COUNTY OF WAYNE, STATE OF MICHIGAN,
 AS RECORDED IN L. 66, P. 82 OF PLATS W.C.R.

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Scale: 1" = 20 Feet Date: 1/08/2024



REVISIONS:

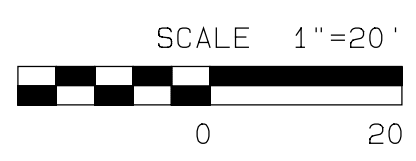
SHEET 5 OF 13
 JOB #: 2024-001

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE LOCATION BASED ON BEST AVAILABLE INFORMATION ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR THE SURVEYING, INC. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.



INDICATES EXISTING ELEVATION — 693.90
 INDICATES PROPOSED GRADE — (693.90)

The existing flood plain line as shown hereon was taken from plan of survey (by others) as shown on sheet 2. Currently the flood plain location and elevation is being re-evaluated to remove the area of development (Building, parking lots and rain gardens) from the floodplain. Once FEMA has determined the new flood plain, these plans will be updated to show new flood plain information.

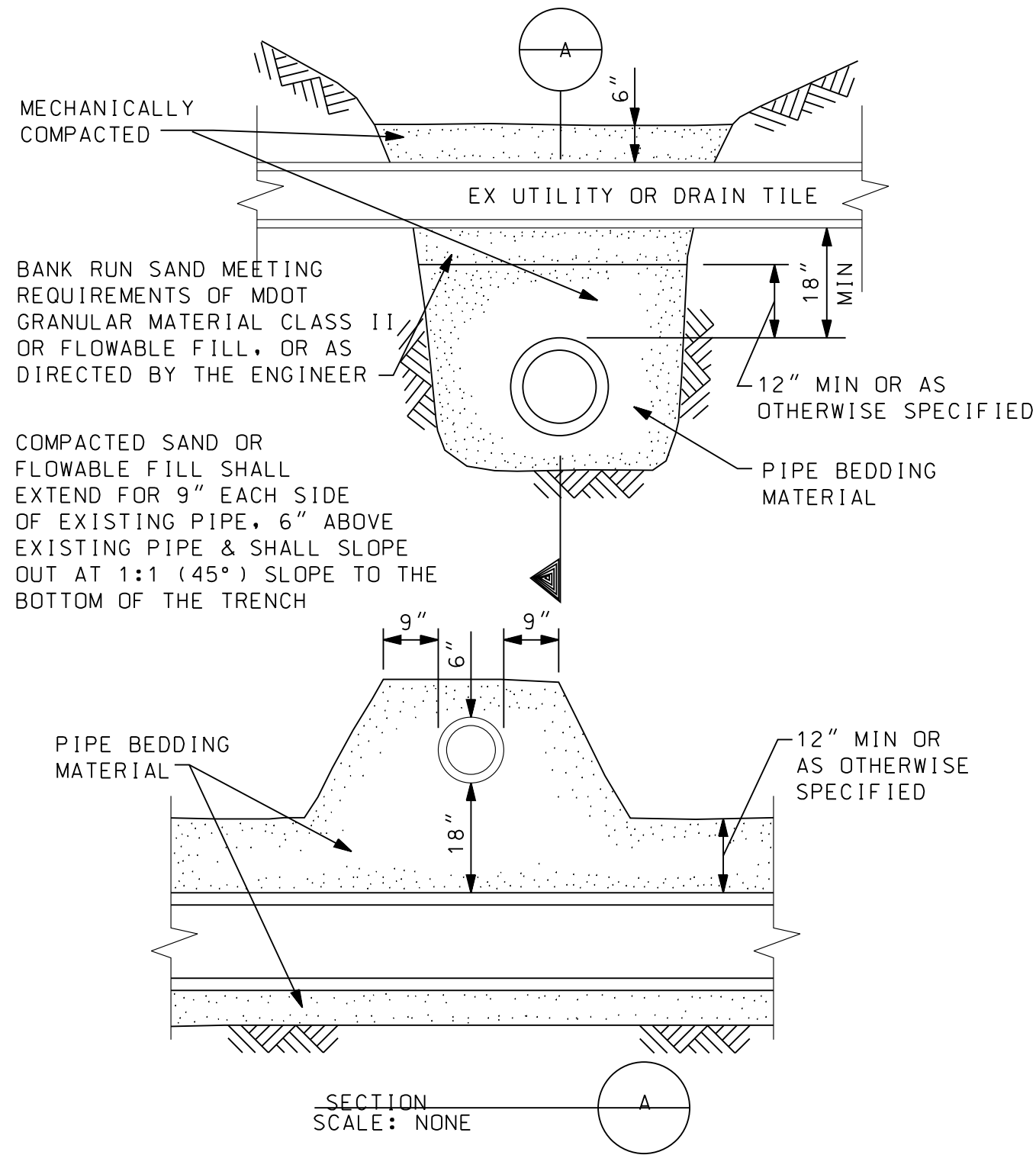




Note Key

1. Brick Enclosure Over Guardrail	12. Heritage Birch Trees - various sizes (8', 10', 12' ht.)
2. Brick Pavers	13. Meadow Lawn
3. Concrete Sidewalk	14. Cut Lawn
4. Sloped Walk for Barrier Free Access	15. Sign with Low Plantings (walker's low catmint)
5. Precast Block Retaining Wall	16. Masonry Dumpster Enclosure
6. Concrete Steps with Handrail	17. Bio - Swale
7. Existing Retaining Wall to Remain	18. HVAC Equipment
8. Bollards	19. Existing Walls
9. No Parking - Loading Zone	20. 'Sunset Red Maple' - various sizes (2", 2 1/2", 3" cal.)
10. Existing Trees / Vegetation	21. 'Pioneer Elm' - various sizes (2", 2 1/2", 3" cal.)
11. Low Shrub Plantings	22. 'Green Mountain' Sugar Maple - various sizes (2", 2 1/2", 3" cal.)

NOTE:
WHERE CONCRETE ENCASEMENT IS SPECIFIED FOR NEW UTILITY A 6" MINIMUM LAYER OF MECHANICALLY COMPACTED SAND SHALL BE MAINTAINED BETWEEN EXISTING UTILITY & TOP OF CONCRETE ENCASEMENT.



STANDARD PIPE SUPPORT
N.T.S.

GENERAL NOTES

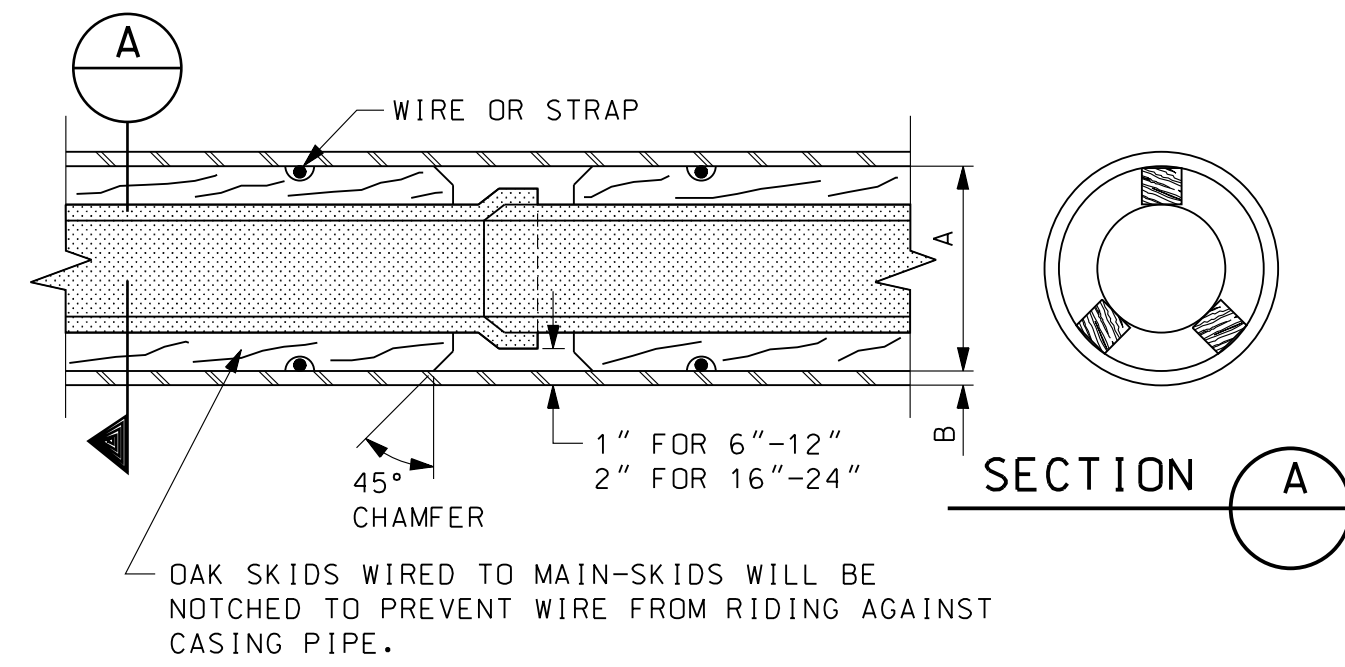
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL ATTEND A RECONSTRUCTION MEETING, AT A TIME AND PLACE AS ARRANGED BY THE COMMUNITY, IN WHICH VARIOUS UTILITY COMPANIES AND GOVERNMENTAL AGENCY REPRESENTATIVES WILL BE PRESENT.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR MUST HAVE IN HIS POSSESSION A COPY OF ALL PERMITS NECESSARY TO CONSTRUCT A CONNECTION TO, OR AN EXTENSION OF, THE WATER SUPPLY, SANITARY SEWER, OR STORM SEWER SYSTEMS.
- THE CONTRACTOR SHALL MAINTAIN HIS CONSTRUCTION OPERATIONS WITHIN THE PRESENTLY EXISTING ROAD RIGHTS-OF-WAY AND EASEMENTS AS NOTED ON THE PLANS THROUGHOUT THE PROJECT. IN THE EVENT THAT THE CONTRACTOR DEEMS IT NECESSARY OR ADVISABLE TO OPERATE BEYOND THE LIMITS OF THE EXISTING RIGHTS-OF-WAY OR EASEMENTS, HE SHALL BE RESPONSIBLE FOR MAKING SPECIAL WRITTEN AGREEMENTS WITH THE PROPERTY OWNERS AND SHALL FURNISH SUCH COPIES OF AGREEMENTS TO THE COMMUNITY AND ENGINEER.
- THE CONTRACTOR SHALL NOTIFY "MISS DIG" (800-482-7171) 3 DAYS (NOT INCLUDING HOLIDAYS OR WEEKENDS) BEFORE STARTING CONSTRUCTION. HE SHALL MAKE ANY NECESSARY ARRANGEMENTS WITH UTILITY COMPANIES FOR RELOCATION OF EXISTING UTILITIES. THESE ARRANGEMENTS SHALL BE MADE IN SUFFICIENT TIME TO ALLOW THE RELOCATION WORK TO BE COMPLETED WITHOUT INTERFERING WITH OR DELAYING THE SEWER CONSTRUCTION.
- THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND THE ENGINEER 48 HOURS PRIOR TO UNCOVERING ANY EXISTING UTILITIES.
- ON ALL WORK WITHIN THE WAYNE COUNTY RIGHT-OF-WAY, THE CONTRACTOR SHALL NOTIFY THE COUNTY ENGINEER AND THE COMMUNITY 72 HOURS PRIOR TO THE START OF ANY CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ALL TRAFFIC AT ALL TIMES AS PER THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- THE CONTRACTOR SHALL AT ALL TIMES PROVIDE EMERGENCY ACCESS TO PROPERTY IN THE VICINITY OF THE CONSTRUCTION FOR POLICE AND FIRE EQUIPMENT, AMBULANCES OR OTHER EMERGENCY VEHICLES TO PROTECT LIFE, HEALTH AND PROPERTY.
- THE CONTRACTOR SHALL MAINTAIN PUBLIC ROADS AFFECTED BY THE CONSTRUCTION OPERATIONS IN A PASSABLE CONDITION UNTIL SUCH TIME AS FINAL RESTORATION OF THESE IMPROVEMENTS CAN BE MADE. IF THE PUBLIC SAFETY IS IN DANGER OR THE NECESSITY EXISTS FOR MAINTAINING TRAFFIC, BACKFILLING MUST BE COMPLETED IMMEDIATELY. IN THE EVENT THAT THE NECESSARY BACKFILL MATERIAL AND EQUIPMENT ARE NOT AVAILABLE WHEN DIRECTION IS GIVEN FOR IMMEDIATE BACKFILL, THE TRENCH SHALL BE BACKFILLED WITH NATIVE MATERIAL TO PROVIDE FOR THE NECESSARY MAINTENANCE OF TRAFFIC AND SAFETY; HOWEVER, THE NATIVE MATERIAL SHALL BE REMOVED WITHIN 48 HOURS AND THE TRENCH PROPERLY BACKFILLED.

GENERAL NOTES CONTINUED

- NO STREET, ROAD OR SECTION THEREOF SHALL BE CLOSED TO THROUGH TRAFFIC UNLESS AUTHORIZED BY THE AGENCY WITH JURISDICTION OVER THE ROADS. PRIOR TO CLOSING A STREET, ROAD, OR SECTION THEREOF, THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A COPY OF A DETOUR PLAN APPROVED BY THE AGENCY HAVING JURISDICTION OVER THE ROADS.
- IN THE EVENT ROADS ARE TO BE CLOSED, THE CONTRACTOR SHALL NOTIFY THE LOCAL FIRE DEPARTMENT, POLICE DEPARTMENT, LOCAL ROAD AUTHORITY, AMBULANCE AND EMERGENCY SERVICES, DEPARTMENT OF PUBLIC WORKS, PUBLIC TRANSIT AUTHORITY, PUBLIC SCHOOL SYSTEM, LOCAL TRASH PICKUP AUTHORITY, AND PUBLIC AND PRIVATE UTILITIES DAILY AS TO WHAT STREETS WILL BE PARTLY BLOCKED OR CLOSED, THE LENGTH OF TIME THE STREETS WILL BE BLOCKED OR CLOSED AND WHEN THE STREETS WILL BE REOPENED TO TRAFFIC.
- PAVED STREETS AND DRIVEWAYS SHALL BE MAINTAINED IN A REASONABLE STATE OF CLEANLINESS AND THE CONTRACTOR SHALL REMOVE ACCUMULATIONS OF DEBRIS CAUSED BY HIS OPERATIONS. THE CONTRACTOR SHALL HAVE, AS A MINIMUM, AN OPERATING SWEEPER BROOM ON THE SITE AT ALL TIMES. THE PAVEMENT SHALL BE CLEANED AT THE CLOSE OF EACH DAYS OPERATION AND AS OFTEN AS NECESSARY BEFORE THAT TIME. FAILURE TO COMPLY SHALL BE CAUSE TO STOP CONSTRUCTION. CONTRACTOR SHALL ALSO COMPLY WITH THE LOCAL AIR POLLUTION CONTROL ORDINANCE.
- ALL GRAVEL AND DIRT ROADS, STREETS OR DRIVEWAYS USED SHALL BE MAINTAINED BY GRADING, PLACING DUST PALLIATIVES, AND MAINTENANCE GRAVEL IN SUFFICIENT QUANTITIES TO ELIMINATE DUST AND MAINTAIN TRAFFIC AS DIRECTED BY THE AGENCY.
- CONTRACTOR SHALL PROVIDE ALL NECESSARY SHEETING, SHORING, DEWATERING, BRACING, TRENCH BOXES, ETC., TO PERFORM WORK SAFELY AND PROTECT EXISTING UTILITIES AND IMPROVEMENTS.
- THE FLOW IN THE EXISTING SEWERS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. 16. CULVERTS, DITCHES, DRAIN TILES, TILE FIELD, DRAINAGE STRUCTURES, ETC., THAT ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE IMMEDIATELY RESTORED.
- CULVERT, DITCHES, DRAIN TILES, TILE FIELDS, DRAINAGE STRUCTURES, ETC. THAT ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE IMMEDIATELY RESTORED.
- ALL PROPERTY IRONS AND MONUMENTS, IF DISTURBED OR DESTROYED BY THE CONTRACTOR'S OPERATION, SHALL BE REPLACED BY A LICENSED LAND SURVEYOR AT THE CONTRACTOR'S EXPENSE.
- AFTER ALL THE PIPE, STRUCTURES, ETC., HAVE BEEN LAID, CONSTRUCTED, AND BACKFILLED, THE SYSTEM SHALL BE TESTED AND FINAL INSPECTED. THE INSPECTION AND TESTING SHALL CONSIST OF A FIRST INSPECTION, TELEVISION INSPECTION (IF APPLICABLE) TESTING, AND FINAL INSPECTION AND MEASUREMENT. THE CONTRACTOR SHALL PROVIDE THE NECESSARY SUPERVISION, LABOR, TOOLS, EQUIPMENT, AND THE MATERIALS NECESSARY FOR THE TESTS WHICH SHALL BE CONDUCTED IN THE PRESENCE OF THE ENGINEER. HE ENGINEER SHALL BE NOTIFIED TWO (2) WORKING DAYS IN ADVANCE OF ALL TESTING.

GENERAL NOTES CONTINUED

- THE FIRST INSPECTION SHALL BE COMPLETED AND ALL REPAIRS MADE IN AMPLE TIME SO THAT THE TELEVISION INSPECTION OF THE UNDERGROUND PORTION OF THE SYSTEM CAN BE COMPLETED WITHIN FOUR (4) WEEKS OF THE COMPLETION OF THE CONSTRUCTION. WHEN RE-TELEVISION IS NECESSARY, AN ADDITIONAL TWO (2) WEEKS WILL BE ALLOWED FOR COMPLETION. TESTING OF THE SYSTEM AS HEREIN DESCRIBED SHALL IMMEDIATELY FOLLOW THE TELEVISION INSPECTION AND SHALL BE COMPLETED WITHIN A TWO (2) WEEK PERIOD.
- FAILURE TO MAINTAIN A SCHEDULE IN COMPLIANCE WITH THESE TERMS WILL AUTOMATICALLY CAUSE THE STOPPAGE OF OTHER WORK AT THE PARTICULAR SITE IN QUESTION UNTIL SUCH TIME AS THE FINAL INSPECTION OF THE COMPLETED UNDERGROUND PORTION OF THE SYSTEM HAS PROGRESSED TO ACCEPTABLE LIMITS.
- THE CONTRACTOR SHALL HAVE THE UNDERGROUND PORTION OF THE SEWER SYSTEM READY FOR THE FIRST INSPECTION WITHIN TWO (2) WEEKS AFTER THE COMPLETION OF UTILITY.
- THE FIRST INSPECTION SHALL CONSIST OF A VISIBLE AND AUDIBLE CHECK OF SEWERS, MANHOLES, GATE WELLS, AND OTHER STRUCTURES TO ASCERTAIN THAT THE STRUCTURE STEPS HAVE BEEN PLACED, ALL LIFT HOLES PLUGGED, THE CHANNELING OF THE MANHOLE BOTTOMS COMPLETED, ALL VISIBLE OR AUDIBLE LEAKS STOPPED, ALL PIPE HAS BEEN PLACED STRAIGHT AND TRUE TO THE PROPER GRADES AND ELEVATION, THE REQUIRED ADJUSTING RINGS AND FRAME AND COVER PROPERTY INSTALLED, ALL TRENCHES AND STRUCTURES BACKFILLED IN AN ACCEPTABLE MANNER AND THAT THE SYSTEM HAS BEEN THOROUGHLY CLEANED.
- THE FIRST INSPECTION SHALL BE CONSIDERED COMPLETED WHEN ALL TREE PAIRS HAVE BEEN MADE AND THE SYSTEM IS READY FOR A TELEVISION INSPECTION AND SUBSEQUENT TESTING.
- TRENCH BACKFILL UNDER ROAD SURFACES, PAVEMENTS, CURBS, DRIVEWAY, SIDEWALK AND WHERE THE TRENCH EDGE IS WITHIN 3- FEET OF THE PAVEMENT SHALL BE TRENCH B (SAND) PER SPECIFICATIONS SECTION 312333. TRENCHING AND BACKFILLING.
- AFTER ALL TESTING, TELEVISION INSPECTION, FINAL RESTORATION AND CLEAN-UP HAS BEEN COMPLETED, A FINAL INSPECTION AND MEASUREMENT WILL BE DONE. THE FINAL INSPECTION SHALL BE REQUESTED BY THE CONTRACTOR AND CONSIST OF, BUT IS NOT LIMITED TO, CHECKING FOR PROPER ALIGNMENT, PROPER GRADE, CLEANLINESS, LEAKS, CONFORMANCE TO THE PLANS AND SPECIFICATION, PROPER STRUCTURAL AND MECHANICAL ADJUSTMENTS, AND RESTORATION. FINAL MEASUREMENT INCLUDES STRUCTURE ELEVATIONS, DISTANCES BETWEEN STRUCTURES, AND CONFIRMATION UTILITIES ARE LOCATED WITHIN EASEMENT AND RIGHT-OF-WAY AREAS.
- SUCCESSFUL COMPLETION OF ANY TEST OR INSPECTION SHALL NOT RELIEVE THE CONTRACTOR FROM THEIR RESPONSIBILITY TO CORRECT ANY DEFICIENCY OR NONCONFORMANCE TO THE PLANS OR SPECIFICATIONS WHICH MAY THEREAFTER BECOME KNOWN.



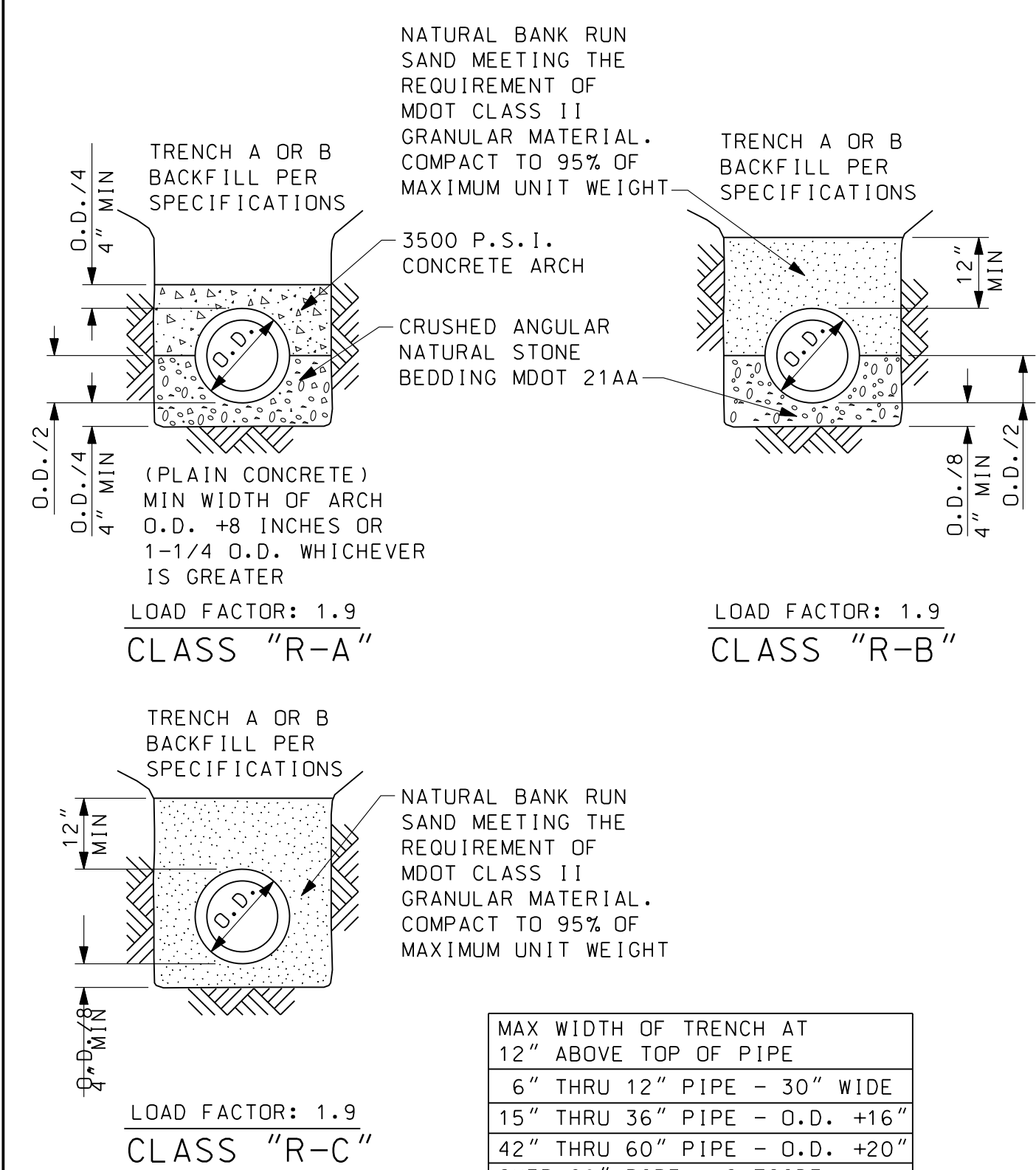
OAK SKIDS WIRED TO MAIN-SKIDS WILL BE NOTCHED TO PREVENT WIRE FROM RIDING AGAINST CASING PIPE.

DIA OF SEWER	DIA OF MAIN	MIN "A"	ROAD CROSSING MIN "B"	RAILROAD CROSSING MIN "B"
6"	16"	.375	.375	.375
8"	16"	.375	.375	.375
12"	20"	.375	.438	.438
10"	16"	.375	.500	.500
12" & 15"	20"	.375	.500	.500
18"	24"	.375	.500	.500

CASING PIPE SHALL BE WELDED STEEL PIPE A.S.T.M. A-252, GR 2 UNLESS OTHERWISE SPECIFIED.

- NOTES:
- NO WATER SHALL BE USED IN BORING UNDER RAILROADS.
 - MAINTAIN MINIMUM OF 5'-6" OF COVER BETWEEN BASE OF RAIL AND TOP OF CASING.
 - THE ENDS OF THE CASING SHALL BE SUITABLY PROTECTED AGAINST THE ENTRANCE OF FOREIGN MATERIAL, BUT SHALL NOT BE TIGHTLY SEALED.
 - WHEN BORING ALL VOIDS OUTSIDE OF CASING PIPE SHALL BE FILLED BY MEANS OF PRESSURE GROUTING WITH 1:3 CEMENT-SAND MORTAR. THIS WORK MUST BE ACCOMPLISHED WITHIN 24 HOURS AFTER THE CROSSING HAS BEEN COMPLETED. BORING SHALL EXTEND A MINIMUM OF 10 FEET OUTSIDE THE EDGES OF THE PAVEMENT.
 - SKIDS ARE TO BE MINIMUM OF 80% OF PIPE LENGTH.
 - CASING SPACERS AS ALLOWED BY THE ENGINEER.

STANDARD CASING SECTION



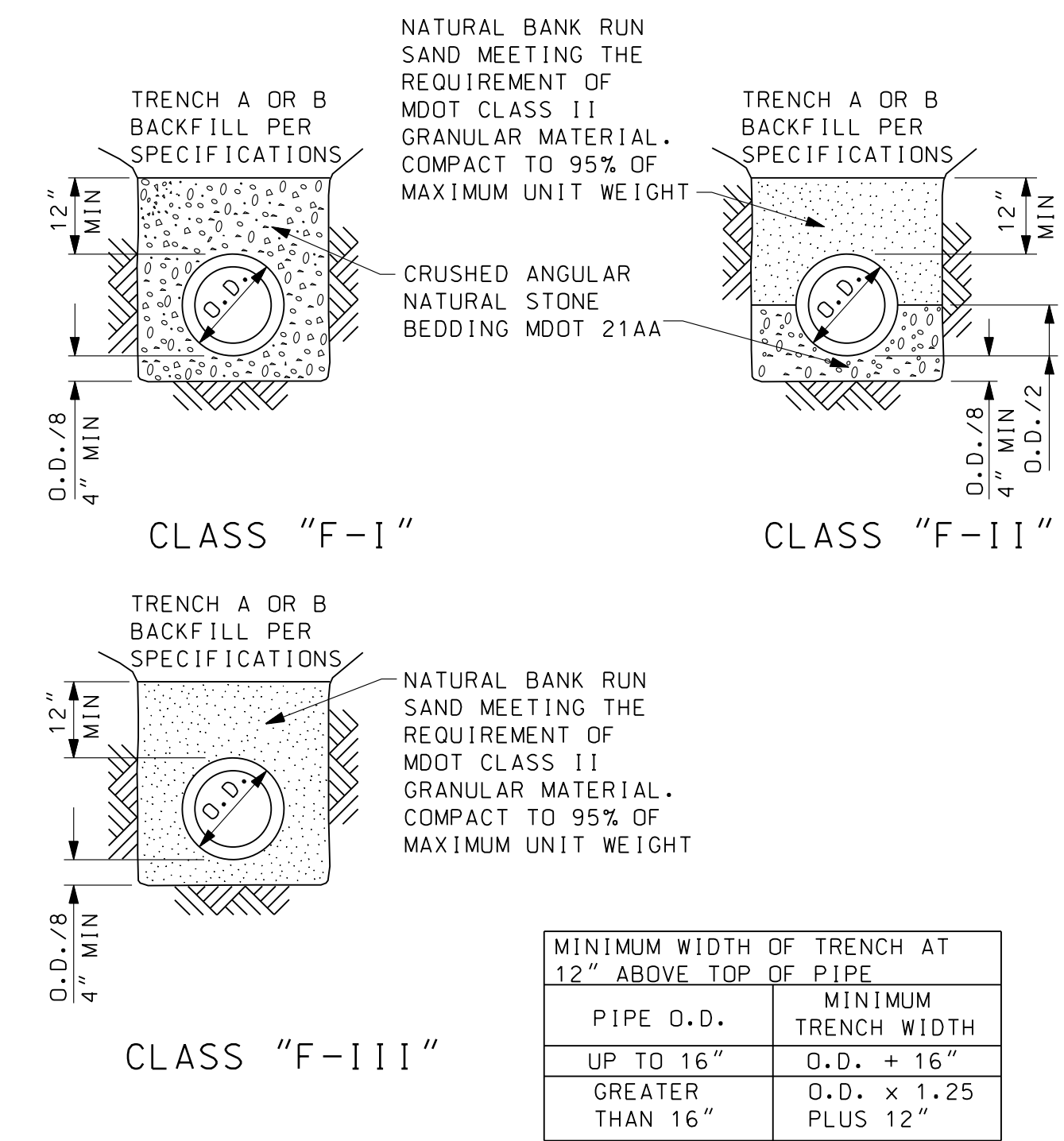
(PLAIN CONCRETE) MIN WIDTH OF ARCH O.D. +8 INCHES OR 1-1/4 O.D. WHICHEVER IS GREATER
LOAD FACTOR: 1.9
CLASS "R-A"

LOAD FACTOR: 1.9
CLASS "R-B"

LOAD FACTOR: 1.9
CLASS "R-C"

MAX WIDTH OF TRENCH AT 12" ABOVE TOP OF PIPE
6" THRU 12" PIPE - 30" WIDE
15" THRU 36" PIPE - O.D. +16"
42" THRU 60" PIPE - O.D. +20"
OVER 60" PIPE - OUTSIDE DIAMETER OF PIPE +24"
MIN WIDTH OF TRENCH 12" ABOVE THE TOP OF PIPE SHALL BE 6" ON EACH SIDE OF PIPE

RIGID PIPE BEDDING DETAILS



CLASS "F-I"

CLASS "F-II"

CLASS "F-III"

MINIMUM WIDTH OF TRENCH AT 12" ABOVE TOP OF PIPE	
PIPE O.D.	MINIMUM TRENCH WIDTH
UP TO 16"	O.D. + 16"
GREATER THAN 16"	O.D. x 1.25 PLUS 12"

FLEXIBLE PIPE BEDDING DETAILS

PROJECT MANAGER: FIELD BOOK INFORMATION: NOT VALID FOR CONSTRUCTION UNLESS SIGNED AND DATED: 2024.10.20 2:02:10 PM BY: m/omagan

NO.	DATE	DESCRIPTION

26251 Northline Rd.
PO Box 10
Taylor, MI 48180
www.wadeftrim.com

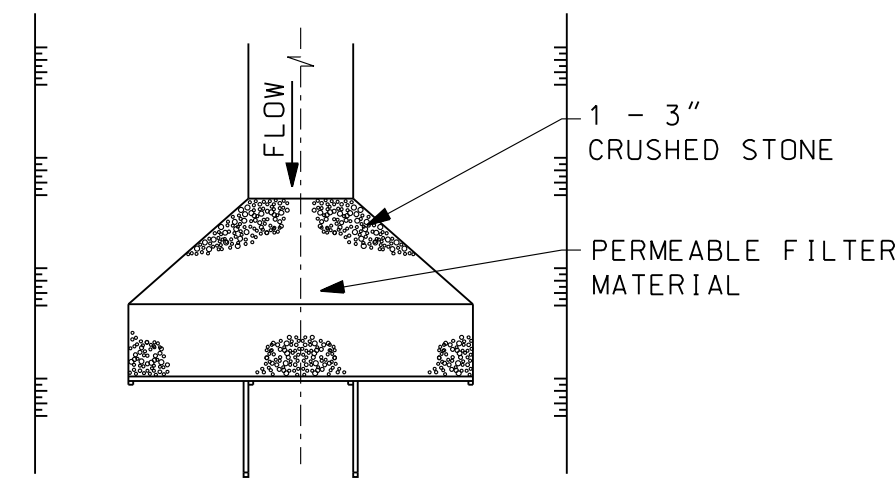
WADE TRIM

CITY OF PLYMOUTH
201 SOUTH MAIN STREET
PLYMOUTH, MICHIGAN 48170

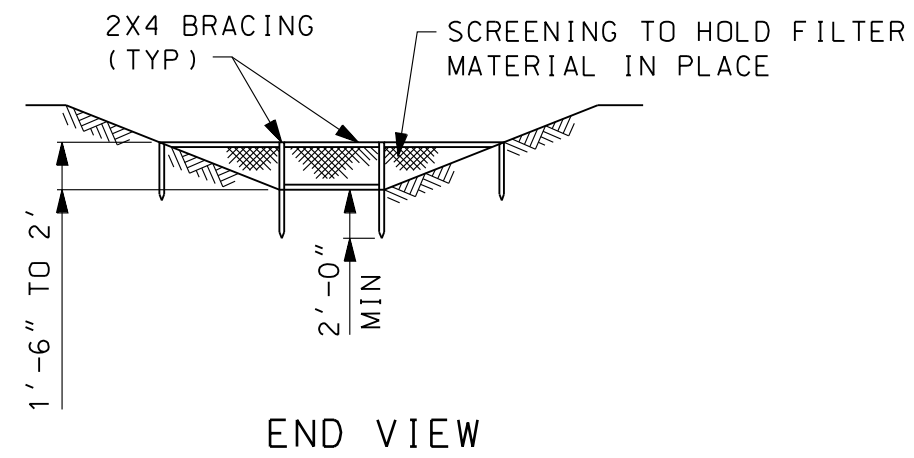
MISCELLANEOUS DETAILS (MD-1)

ISSUED FOR:	DATE:	BY:
JOB NO.		
SHEET		

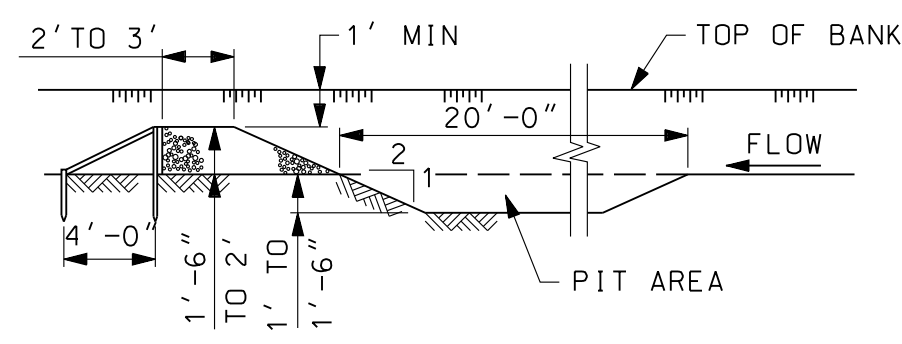
MD-1



PLAN VIEW



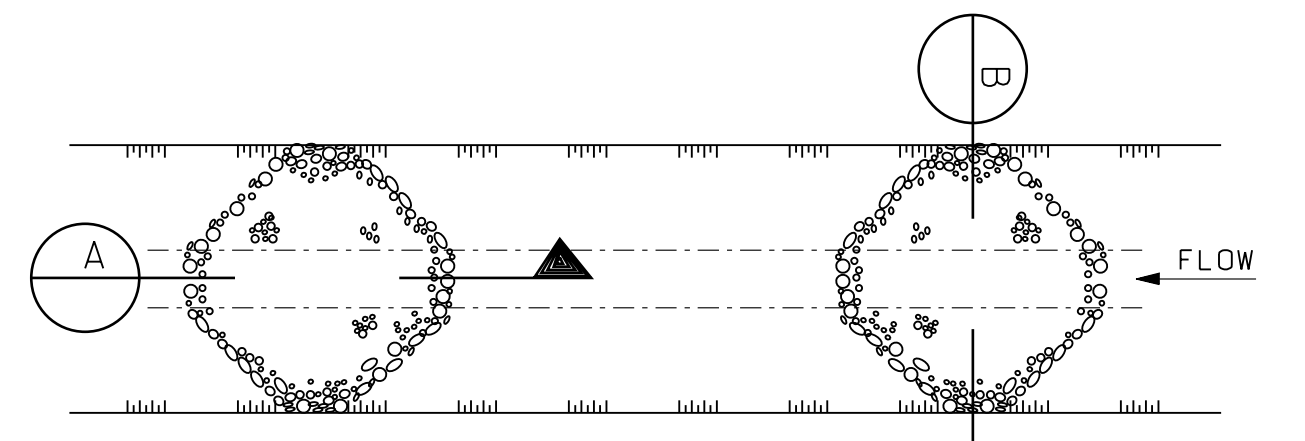
END VIEW



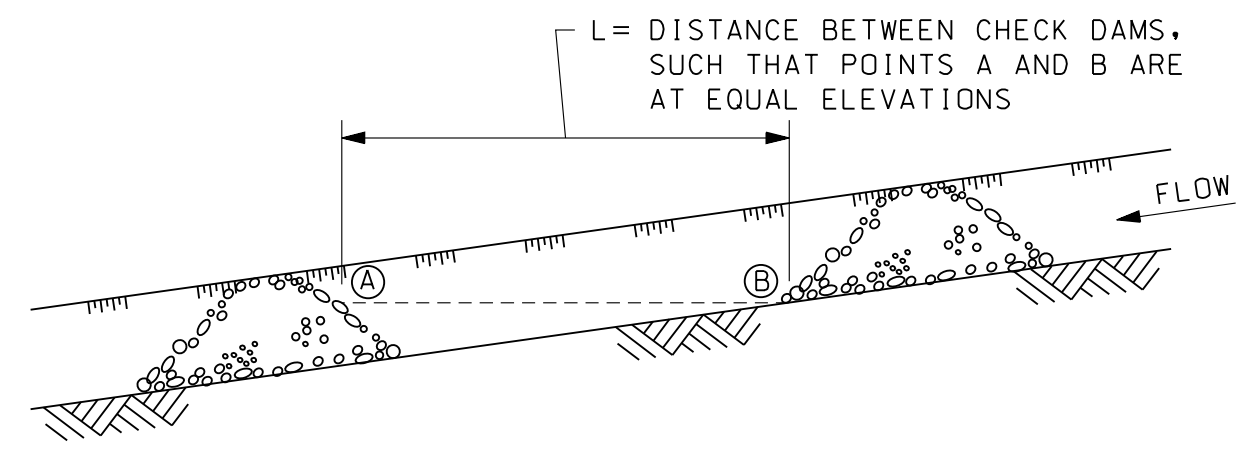
SECTION

- NOTES:
1. PLACE DITCH SEDIMENT TRAP PRIOR TO ON-SITE CONSTRUCTION WORK.
 2. MAINTAIN DITCH SEDIMENT TRAP IN GOOD OPERATION DURING PROJECT & REMOVE AT END OF PROJECT.
 3. CHANNEL TO BE RESTORED TO ORIGINAL CONDITION OR BETTER AS DIRECTED BY THE ENGINEER.
 4. CRUSHED STONE INCLUDED WITH DITCH SEDIMENT TRAP.

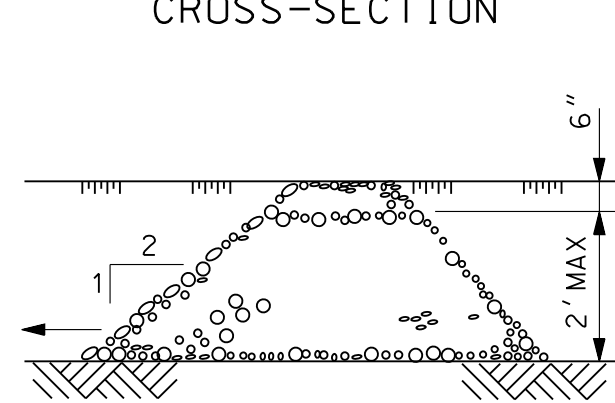
DITCH SEDIMENT TRAP
NOT TO SCALE



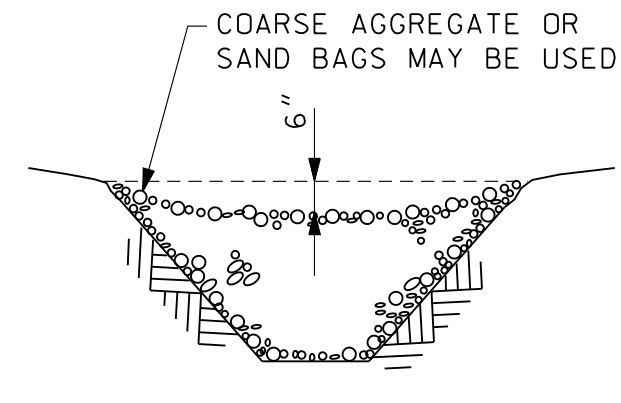
PLAN VIEW



CROSS-SECTION



SECTION A-A



SECTION B-B

- NOTE:
- CHECK DAMS GREATER THAN TWO FEET IN DEPTH MAY SERIOUSLY IMPACT THE FLOW CHARACTERISTICS OF THE DITCH.

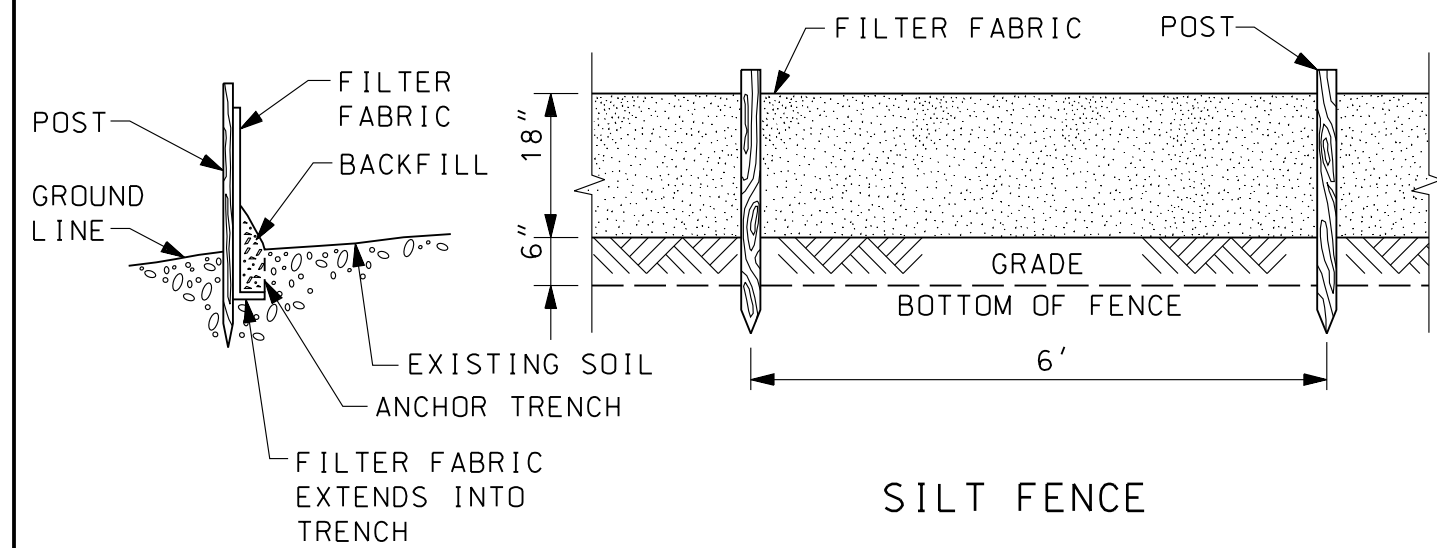
CHECK DAMS
NOT TO SCALE

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

1. ALL EROSION AND SEDIMENT CONTROL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE AGENCY HAVING JURISDICTION.
2. ALL SOIL EROSION CONTROL MEASURES SHALL BE CHECKED A MINIMUM OF ONCE PER WEEK AND WITHIN A MINIMUM OF 24 HOURS AFTER EVERY RAINFALL. ANY SOIL EROSION CONTROL MEASURES DAMAGED OR RENDERED INEFFECTIVE SHALL BE IMMEDIATELY REPAIRED OR REMOVED AND REPLACED AT NO ADDITIONAL COST.
3. EROSION AND ANY SEDIMENT FROM WORK ON THIS SITE SHALL BE CONTAINED ON THE SITE AND NOT ALLOWED TO COLLECT ON ANY OFF-SITE AREAS OR IN WATERWAYS. WATERWAYS INCLUDE BOTH NATURAL AND MANMADE OPEN DITCHES, STREAMS, STORM DRAINS, LAKES, AND PONDS.
4. ALL SOIL EROSION AND SEDIMENTATION CONTROL (SESC) DEVICES SHALL BE INSTALLED PRIOR TO CONTRACTOR BEGINNING ANY WORK. ALL SESC DEVICES SHALL BE MAINTAINED IN AN EFFECTIVE, FUNCTIONING CONDITION AT ALL TIMES DURING THE COURSE OF THE WORK. ALL TEMPORARY SESC DEVICES SHALL BE REMOVED AND THE AREA RESTORED AFTER THE PERMANENT SESC MEASURES ARE INSTALLED AND FUNCTIONING.
5. DEBRIS FROM THE PROJECT SHALL BE LEFT ON THE SITE BY DELIVERY OR CONSTRUCTION VEHICLES THROUGH THE USE OF CLEAN STONE EXITS. SHOULD THE STONE BECOME INEFFECTIVE IT WILL BE REPLACED. ALL CONSTRUCTION TRAFFIC WILL USE THE CLEAN STONE EXITS.
6. EARTH EMBANKMENT BRIDGES PLACED OVER NEW PAVEMENT SHALL BE LOCATED ONLY AT PAVEMENT HIGH-POINTS AND SHALL HAVE STRAW BALES PLACED ALONG EACH SIDE OF THE BRIDGE FOR THE WIDTH OF THE PAVEMENT.
7. IMMEDIATELY AFTER SEEDING, MULCH ALL SEEDING AREAS WITH UNWEATHERED SMALL GRAIN STRAW OR HAY SPREAD UNIFORMLY AT THE RATE OF 1-1/2 TO 2 TONS PER ACRE. ANCHOR MULCH WITH DISC-TYPE MULCH ANCHORING TOOL OR OTHER MEANS AS APPROVED BY THE AGENCY WITH JURISDICTION.
8. ALL MUD, DIRT, AND DEBRIS TRACKED OR SPILLED ONTO EXISTING ROADS FROM THIS SITE SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR.
9. PERMANENT SOIL EROSION CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, OR ANY DISTURBED LAND AREA SHALL BE COMPLETED WITHIN 15 CALENDAR DAYS AFTER FINAL GRADING OR FINAL EARTH CHANGES HAVE BEEN COMPLETED. WHEN IT IS NOT POSSIBLE TO PERMANENTLY STABILIZE A DISTURBED AREA AFTER AN EARTH CHANGE HAS BEEN COMPLETED OR WHERE SIGNIFICANT EARTH CHANGE ACTIVITY EASES, TEMPORARY SOIL EROSION CONTROL MEASURES SHALL BE IMPLEMENTED IMMEDIATELY. ALL TEMPORARY SOIL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE ESTABLISHED. ALL PERMANENT SOIL EROSION CONTROL MEASURES WILL BE IMPLEMENTED AND ESTABLISHED BEFORE A CERTIFICATE OF COMPLIANCE IS ISSUED. ALL DRAIN BANKS DISTURBED BY CONSTRUCTION SHALL BE RESTORED WITHIN FIVE CALENDAR DAYS AND SOD PEGGED IN PLACE.

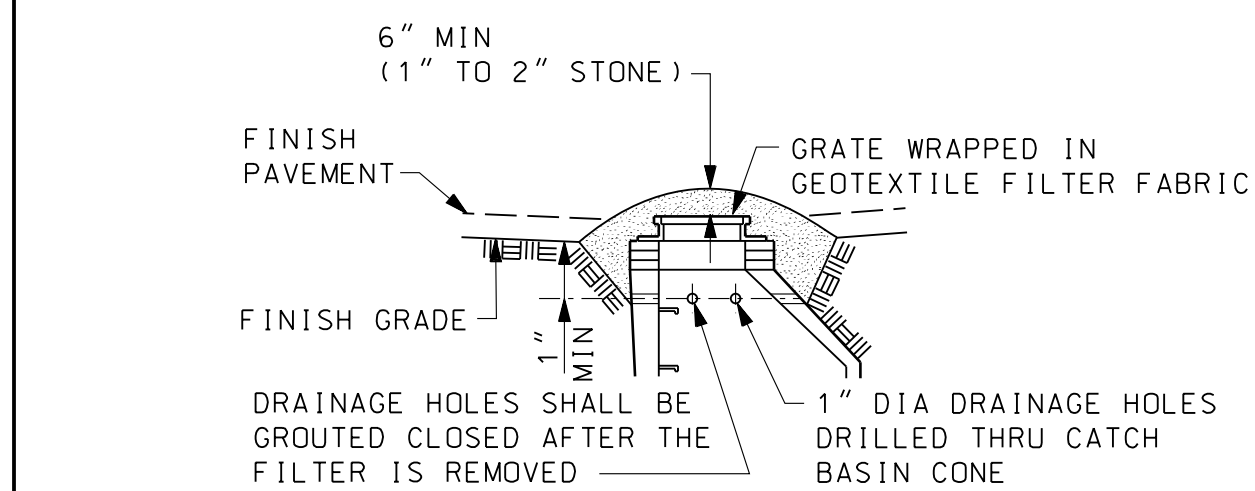
SOIL EROSION AND SEDIMENTATION CONTROL NOTES

10. PARTICULAR CARE SHOULD BE TAKEN WHEN WORKING ALONG THE PERIMETER OF THE SITE. IN NO EVENT SHALL WORK AREA EXTEND BEYOND THE LIMITS INDICATED ON THE PLANS.
11. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO DEWATER THE GROUND IN THE COURSE OF CONSTRUCTING THE PROPOSED UTILITY, THE CONTRACTOR SHALL CONSTRUCT A TEMPORARY SOIL EROSION CONTROL DEVICE IN A MANNER THAT WILL FILTER ALL DISCHARGED WATER FROM THE DEWATERING OPERATION. IN NO INSTANCE SHALL THE DEWATERING DISCHARGE BE PERMITTED TO FLOW UNFILTERED FROM THE CONSTRUCTION SITE.
12. THE CONTRACTOR SHALL CONTROL THE DUST ON THE SITE DURING THE LIFE OF THE CONTRACT. IN ACCORDANCE WITH THE SPECIFICATIONS AND THE REQUIREMENTS OF THE COMMUNITY THIS DUST CONTROL SHALL BE ACCOMPLISHED BY THE APPLICATION OF A POSITIVE DUST PICK-UP METHOD WITH WATER ON HARD SURFACES. SUCH DUST CONTROL MATERIALS SHALL BE APPLIED AS OFTEN AS IS NECESSARY IN THE OPINION OF THE COMMUNITY TO CONTROL THE DUST.
13. SHOULD THE SOIL EROSION CONTROL REQUIREMENTS BE NEGLECTED OR NOT ADEQUATELY FOLLOWED, THE COMMUNITY MAY REQUIRE THE CONTRACTOR TO CEASE CONSTRUCTION OPERATIONS AND TO APPLY HIS ENTIRE FORCE TO MEET THE REQUIREMENTS BEFORE PROCEEDING FURTHER WITH THE PROJECT.
14. SOIL EROSION AND SEDIMENTATION CONTROL SHALL BE IN ACCORDANCE WITH PART 91 SOIL EROSION AND SEDIMENTATION CONTROL (SESC), OF THE NATURAL RESOURCES AND ENVIRONMENTAL PROTECTION ACT, 1994 PA 451, AS AMENDED (NREPA).
15. AS SOON AS POSSIBLE, COMPLETE FINAL GRADING AND PLACING OF PERMANENT SOIL EROSION CONTROL DEVICES. AFTER ESTABLISHMENT OF PERMANENT VEGETATION, REMOVE ALL TEMPORARY SOIL EROSION CONTROL MEASURES.
16. SOIL EROSION AND SEDIMENTATION CONTROL IS UNDER THE JURISDICTION OF THE CITY OF PLYMOUTH.

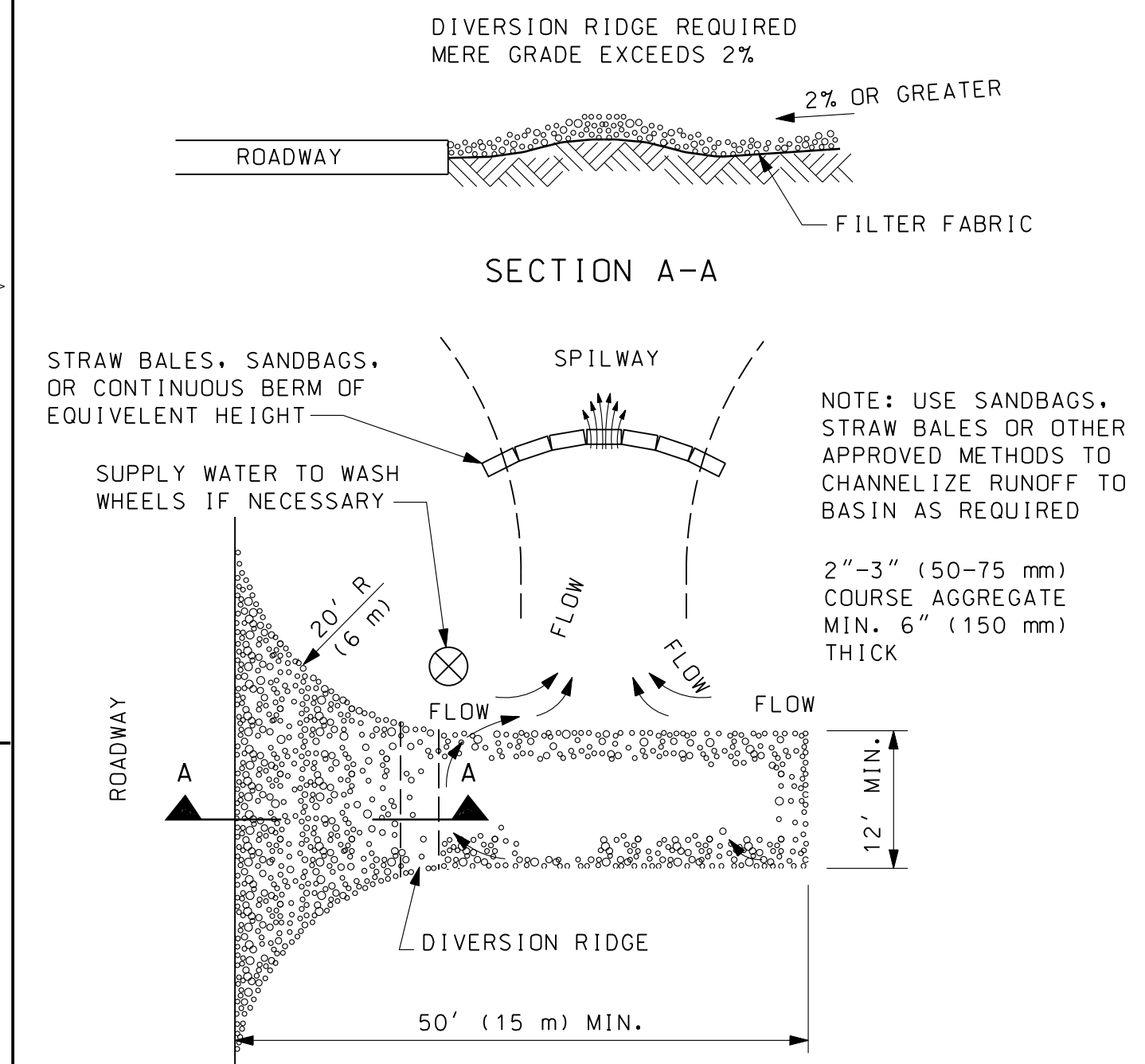


SILT FENCE

SILT FENCE
NOT TO SCALE



DRAINAGE STRUCTURE FILTER
NOT TO SCALE



- NOTE:
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
 2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
 3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.

TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT
NOT TO SCALE

SEQUENCE OF CONSTRUCTION - SESC

1. INSTALL ALL TEMPORARY SOIL EROSION CONTROL MEASURES PRIOR TO DISTURBING ANY EARTH ON THE SITE.
2. INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT DRIVE PRIOR TO DISTURBING ANY EARTH ON SITE. ALL TRUCKS LEAVING THE CONSTRUCTION SITE SHALL PASS THROUGH A TEMPORARY GRAVEL CONSTRUCTION ENTRANCE/EXIT DRIVE TO REMOVE DIRT AND SEDIMENT. ANY DIRT AND ACCUMULATED SEDIMENT ON ROADS AND STREETS IN THE VICINITY OF THE PROJECT SHALL BE SWEEPED CLEAN AT LEAST TWICE DAILY WITH A VACUUM TYPE PICKUP BROOM.
3. STABILIZE SLOPES STEEPER THAN 1 ON 4, CHANNELS AND SWALES WITHIN 7 DAYS OF EARTH DISTURBANCE. INSTALL PERMANENT STABILIZATION MEASURES WITHIN 5 DAYS OF FINAL GRADING.
4. DURING STORM SEWER INSTALLATION, ALL NEWLY CONSTRUCTED DRAINAGE STRUCTURES SHALL BE PROTECTED WITH A DRAINAGE STRUCTURE FILTER. THIS WORK WILL BE INCLUDED IN THE DRAINAGE COST.
5. INSTALL TOPSOIL, SEED AND MULCH / TOPSOIL AND SOD HYDROSEED ON DISTURBED RIGHT-OF-WAY WITHIN 5 DAYS OF COMPLETING UTILITY INSTALLATION.
6. PLACE RIPRAP WITHIN 24 HOURS OF PLACING CULVERTS, HEADWALLS OR OTHER DRAINAGE INLETS/OUTLETS.
7. CLEAN ALL ACCUMULATED SEDIMENT FROM CATCH BASINS, SEWERS AND PAVEMENT AREAS AS REQUIRED FOLLOWING COMPLETION OF CONSTRUCTION.
8. THE EXACT SCHEDULE OF SOIL EROSION AND SEDIMENTATION CONTROL EVENTS (WITH DAYS AND/OR DATES OF THE VARIOUS ACTIVITIES) SHALL BE SUBMITTED TO (WAYNE COUNTY DEPARTMENT OF ENVIRONMENT, AND LAND RESOURCE MANAGEMENT DIVISION) BY THE CONTRACTOR, FOR REVIEW AND APPROVAL, PRIOR TO OBTAINING A PERMIT.

PROJECT MANAGER: FIELD BOOK INFORMATION: PROJECT: 201 SOUTH MAIN STREET PLYMOUTH, MICHIGAN 48170 DATE: 11/17/2024 2:07:36 PM BY: m/amsagan

REV.	DATE	DESCRIPTION	BY

NOT VALID FOR CONSTRUCTION UNLESS SIGNED AND DATED:

26251 Northline Rd.
PO Box 10
Taylor, MI 48180
www.wadefirm.com

CITY OF PLYMOUTH
201 SOUTH MAIN STREET
PLYMOUTH, MICHIGAN 48170

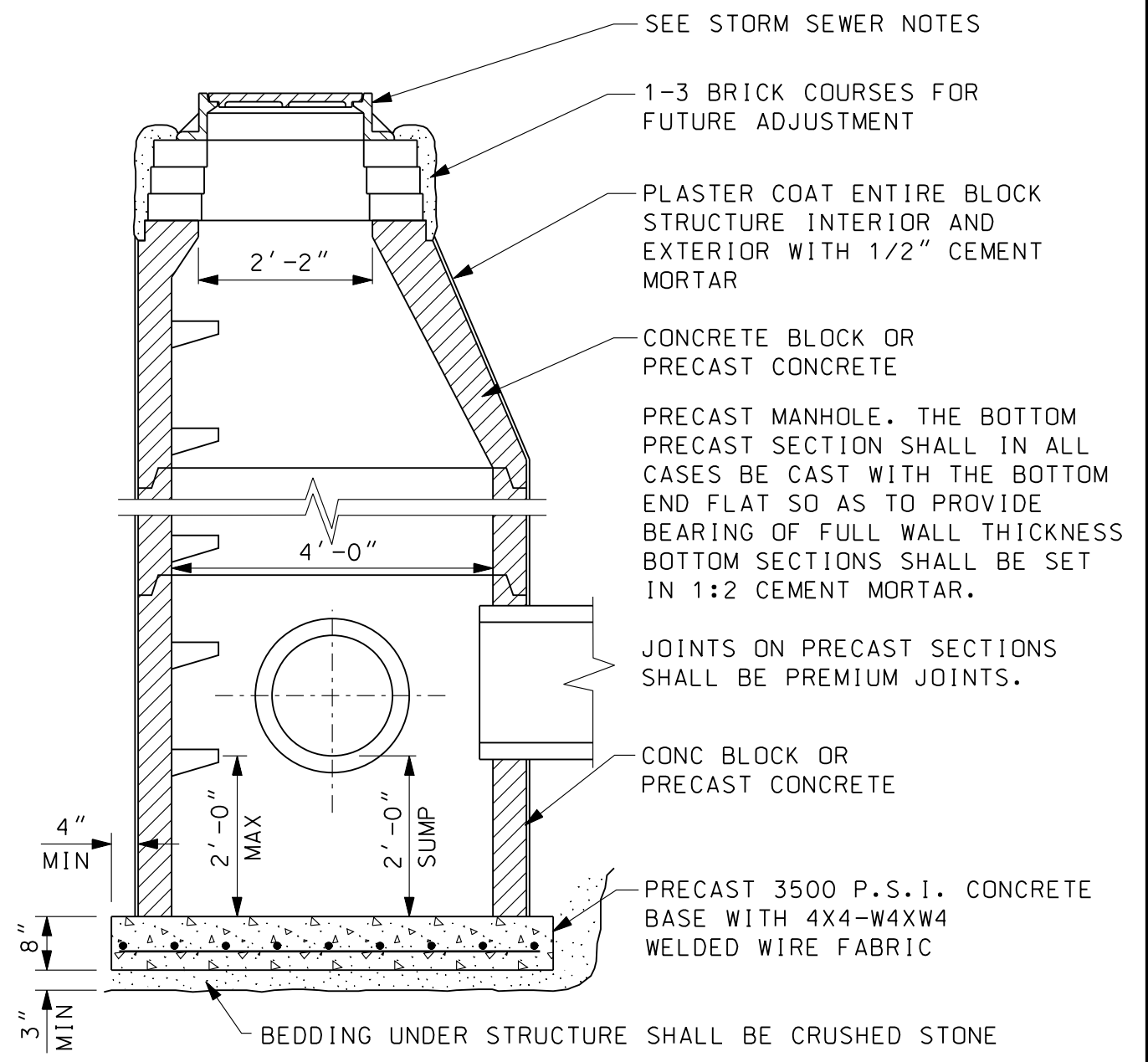
STANDARD SOIL EROSION AND SEDIMENTATION CONTROL DETAILS (SE-1)

ISSUED FOR: DATE: BY:

JOB NO. -

SHEET SE-1

© Wade Trim Group, Inc. STANDARD SOIL EROSION AND SEDIMENTATION CONTROL DETAILS (SE-1 OF 1)



SEE STORM SEWER NOTES

1-3 BRICK COURSES FOR FUTURE ADJUSTMENT

PLASTER COAT ENTIRE BLOCK STRUCTURE INTERIOR AND EXTERIOR WITH 1/2" CEMENT MORTAR

CONCRETE BLOCK OR PRECAST CONCRETE

PRECAST MANHOLE. THE BOTTOM PRECAST SECTION SHALL IN ALL CASES BE CAST WITH THE BOTTOM END FLAT SO AS TO PROVIDE BEARING OF FULL WALL THICKNESS. BOTTOM SECTIONS SHALL BE SET IN 1:2 CEMENT MORTAR.

JOINTS ON PRECAST SECTIONS SHALL BE PREMIUM JOINTS.

CONC BLOCK OR PRECAST CONCRETE

PRECAST 3500 P.S.I. CONCRETE BASE WITH 4X4-W4XW4 WELDED WIRE FABRIC

BEDDING UNDER STRUCTURE SHALL BE CRUSHED STONE

M.H. STEPS SHALL BE SPACED AS FOLLOWS:
 CONC BLK. M.H. 18" CENTERS
 PRECAST M.H. 16" CENTERS

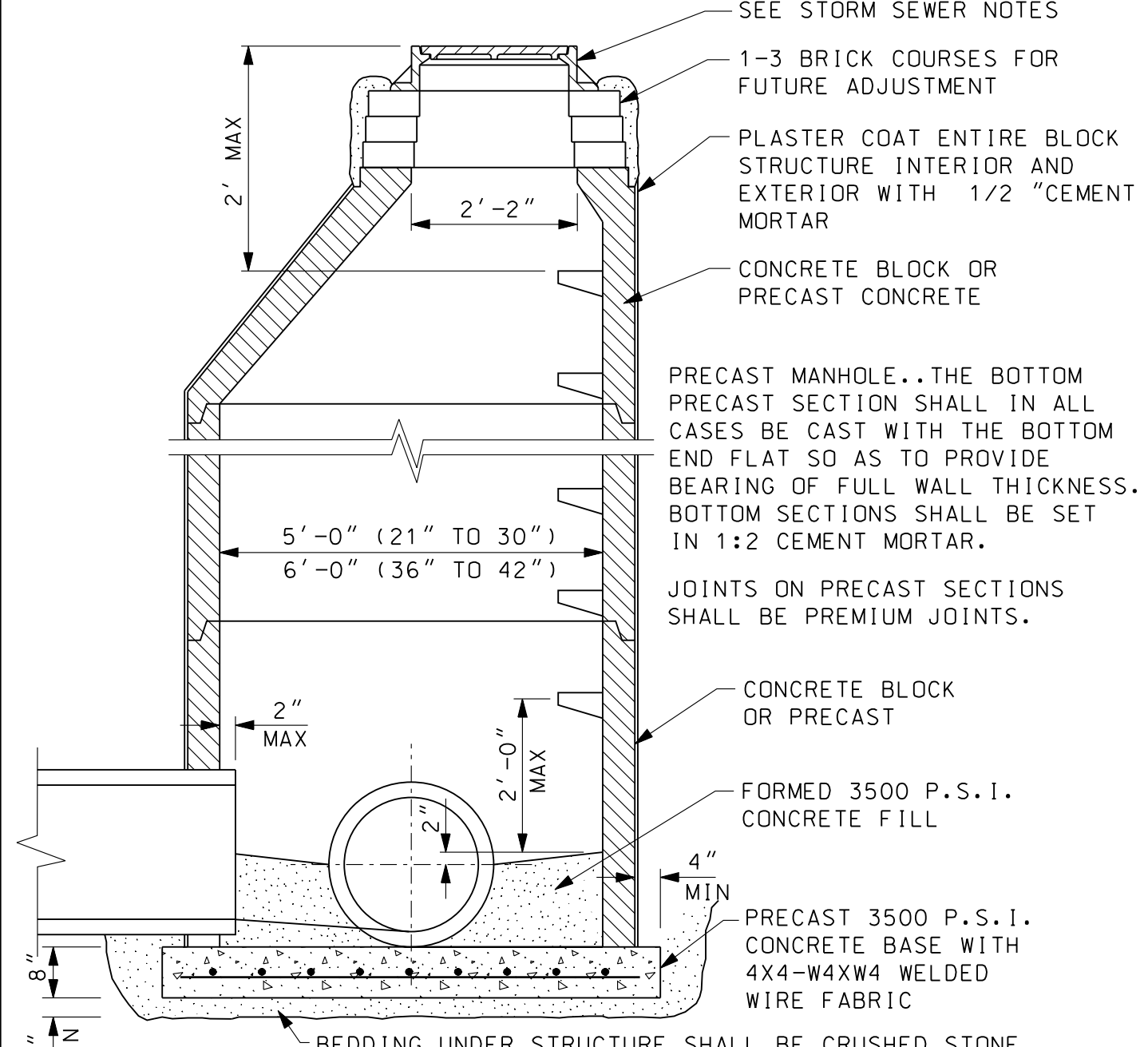
TOP STEP TO BE 24" MAX BELOW TOP OF FRAME

MIN CONE HEIGHTS AS FOLLOWS:
 BLOCK CONCENTRIC 3'-0"
 PRECAST ECCENTRIC 2'-8" OR 3'-4"

CONC BLOCK MANHOLE:
 APPROVED CONC BLOCK
 USE 6" OR 8" MIN THICK BLOCK TO 16' DEPTH USE 12" MIN THICK BLOCK TO 24' DEPTH 16" MIN BLOCK WALL BELOW 24' DEPTH

PRECAST MANHOLE:
 ASTM C-478 UP TO 32' DEEP WITH 5" MIN THICK WALL

STANDARD STORM MANHOLE FOR SEWERS WITH OUTLETS OF 18" & UNDER
N.T.S.



SEE STORM SEWER NOTES

1-3 BRICK COURSES FOR FUTURE ADJUSTMENT

PLASTER COAT ENTIRE BLOCK STRUCTURE INTERIOR AND EXTERIOR WITH 1/2" CEMENT MORTAR

CONCRETE BLOCK OR PRECAST CONCRETE

PRECAST MANHOLE. THE BOTTOM PRECAST SECTION SHALL IN ALL CASES BE CAST WITH THE BOTTOM END FLAT SO AS TO PROVIDE BEARING OF FULL WALL THICKNESS. BOTTOM SECTIONS SHALL BE SET IN 1:2 CEMENT MORTAR.

JOINTS ON PRECAST SECTIONS SHALL BE PREMIUM JOINTS.

CONCRETE BLOCK OR PRECAST

FORMED 3500 P.S.I. CONCRETE FILL

PRECAST 3500 P.S.I. CONCRETE BASE WITH 4X4-W4XW4 WELDED WIRE FABRIC

BEDDING UNDER STRUCTURE SHALL BE CRUSHED STONE.

M.H. STEPS SHALL BE SPACED AS FOLLOWS:
 CONC BLK. M.H. 18" CENTERS
 PRECAST M.H. 16" CENTERS

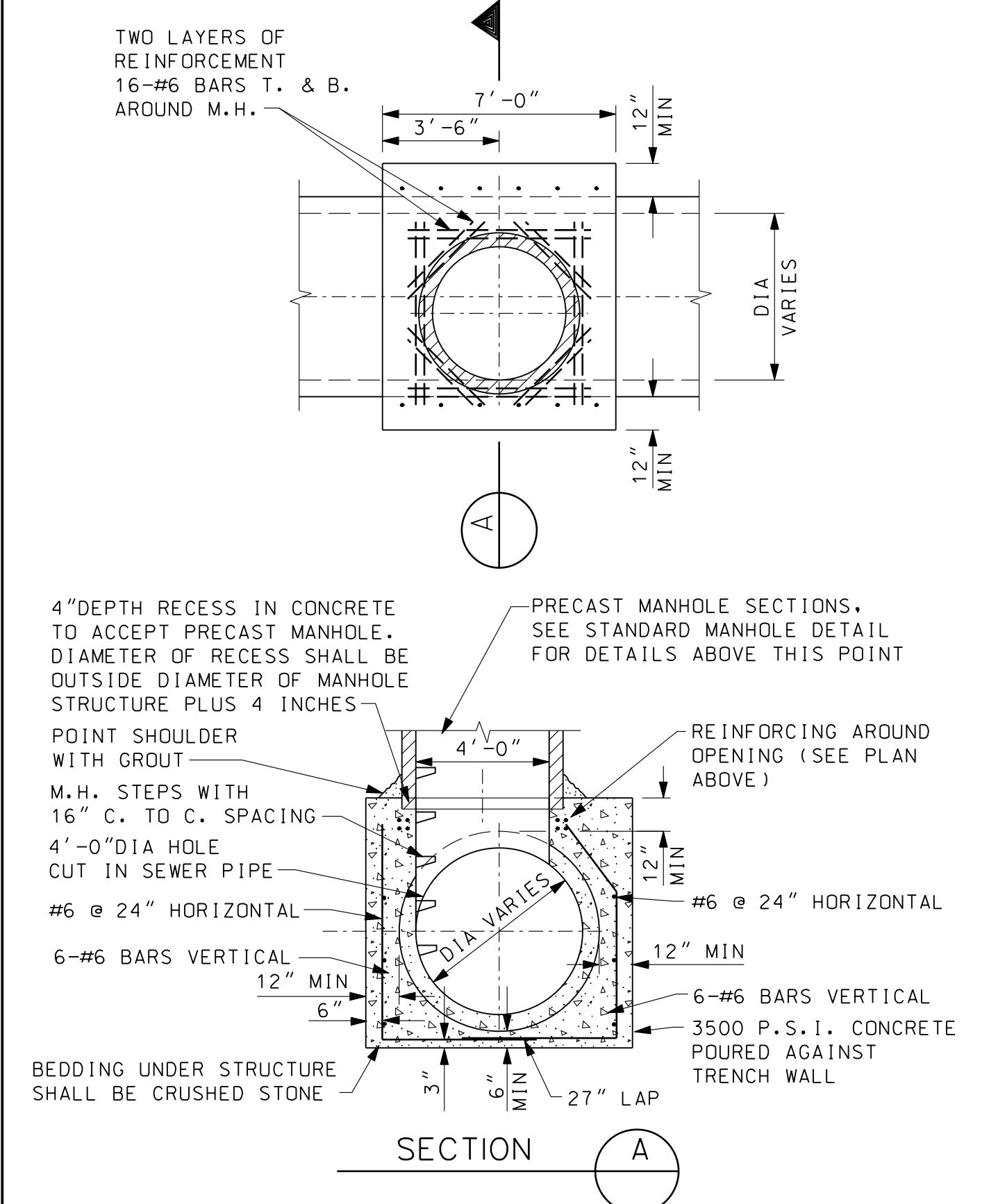
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 BLOCK CONCENTRIC 3'-0"
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 ASTM C-478 UP TO 32' DEEP WITH 5" MIN THICK WALL

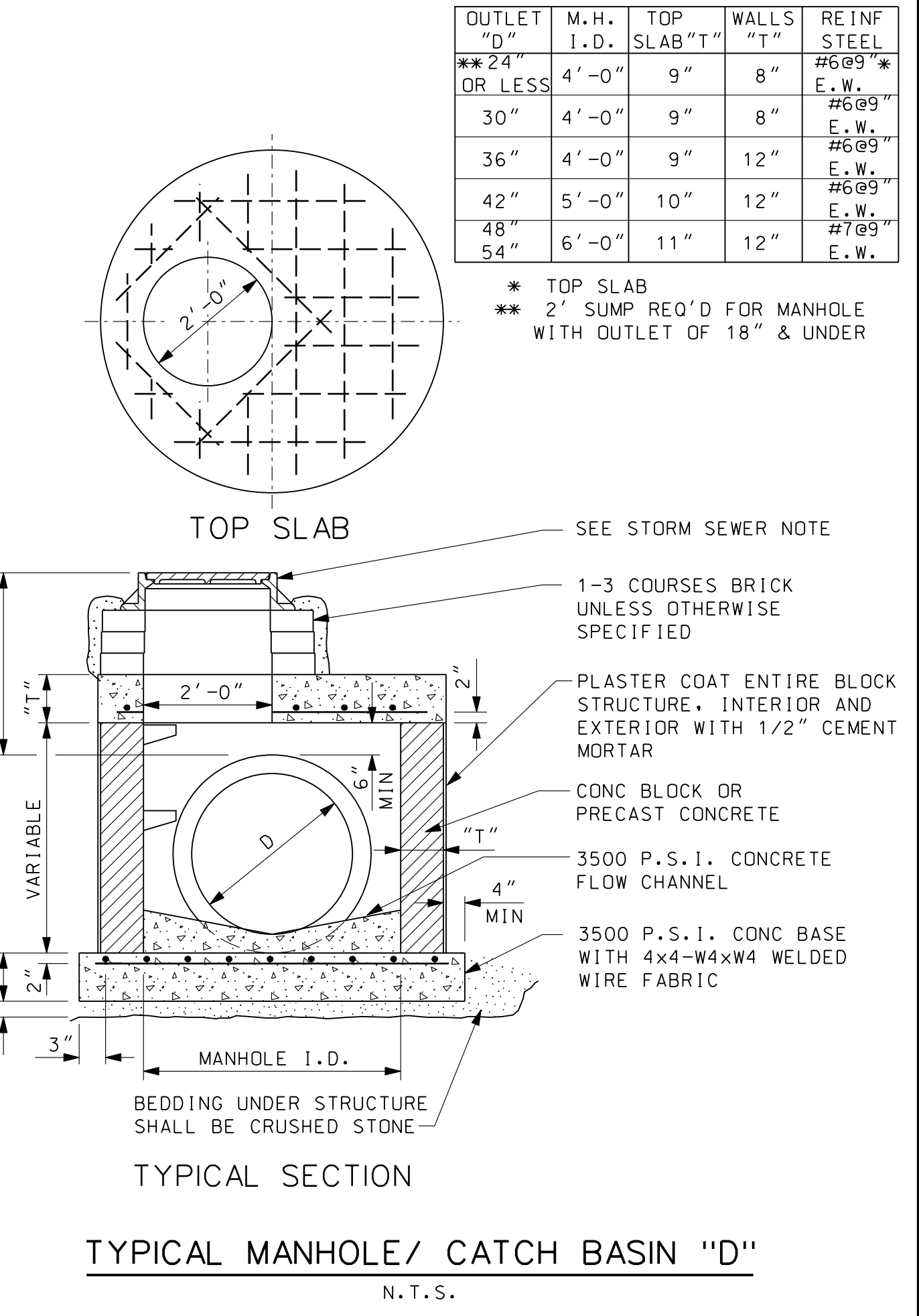
STANDARD STORM MANHOLE FOR SEWERS 21" TO 42"
N.T.S.



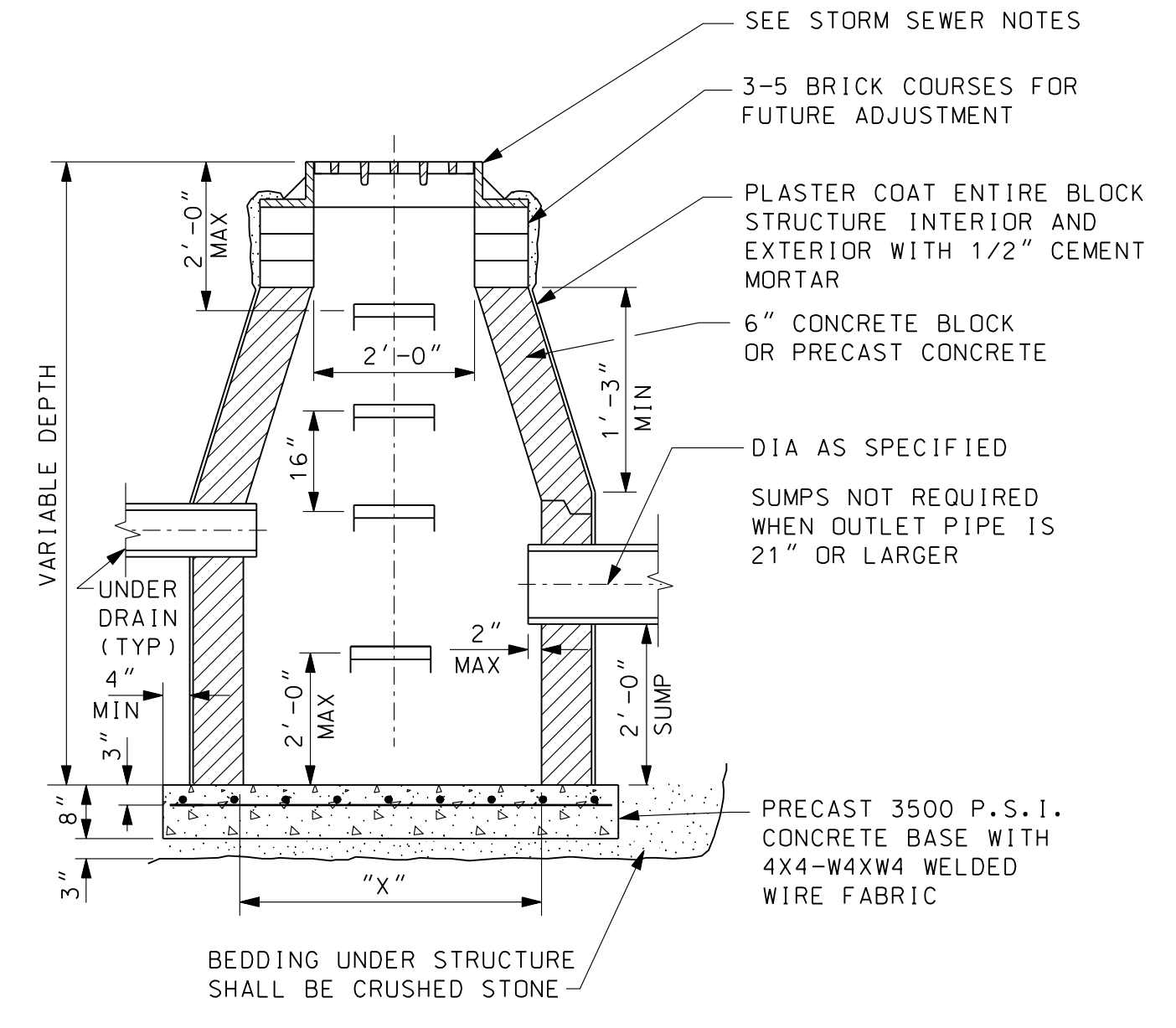
STANDARD STORM MANHOLE FOR 48" & LARGER SEWERS
N.T.S.

- STORM SEWER NOTES**
- ALL STORM SEWER CONSTRUCTION SHALL CONFORM TO THE CURRENT STANDARDS AND GENERAL SPECIFICATION OF THE AGENCY OR AGENCIES HAVING JURISDICTION OF THE STORM SEWER AND CONSTRUCTION AREA.
 - DETAILS ARE FOR STRUCTURES WITH NO MORE THAN TWO PIPES. 180° APART. LARGER DIAMETER STRUCTURES MAY BE REQUIRED FOR DIFFERENT CONFIGURATIONS.
 - ALL STRUCTURES REQUIRE A MINIMUM OF 8-INCHES OF WALL BETWEEN PIPE OPENINGS. LARGER DIAMETER STRUCTURES MAY BE REQUIRED WHERE PIPE ENTERING THE STRUCTURE ARE LESS THAN 90° APART IN ANY DIRECTION.
 - ALL CASTING RIMS SHALL BE SET TO GRADE OR AS SHOWN ON THE PLANS.
 - NO MANHOLES OR OTHER STRUCTURES MAY BE INSTALLED WITHIN DRIVEWAYS, DRIVE APPROACHES, OR SIDEWALKS.
 - ALL CATCH BASIN STORM PIPE SHALL BE 12-INCH DIAMETER C76, CL-IV CONCRETE UNLESS OTHERWISE NOTED.
 - MANHOLE STEPS TO BE GRAY IRON OR STEEL REINFORCED POLYPROPYLENE ASTM 2146, TYPE II, GRADE 49108.
 - CATCH BASIN AND INLET FRAME AND COVERS SHALL BE SPECIFIED AS FOLLOWS:
 - WHEN LOCATED IN PAVEMENT GUTTER LINE, FRAME AND COVER SHALL BE E.J.I.W. NO. 5080, NEENAH R-3448-C TYPE "A" (RECTANGULAR), OR EQUIV.
 - WHEN LOCATED IN PAVED AREAS OTHER THAN GUTTER LINE, FRAME SHALL BE E.J.I.W. NO. 1040 WITH TYPE "M1" COVER, NEENAH R-2370 TYPE "G" COVER, OR EQUIV.
 - WHEN LOCATED IN YARD AREAS, FRAMES SHALL BE E.J.I.W. NO. 1000 WITH TYPE "N" OR "M" COVER, NEENAH R-2370 TYPE "D" OR "B" COVER, OR EQUIV.
 - MANHOLE FRAME AND COVER SHALL BE E.J.I.W. NO. 1040 OR NEENAH R-1642 WITH SOLID COVER OR EQUAL.
 - DIFFERENTIAL OF EXCAVATION AROUND EXISTING MANHOLES SHALL NOT EXCEED SIX FEET.
 - PLACE SAND BACKFILL WITHIN THREE FEET OF ALL STRUCTURES.
 - ALL STORM SEWER PIPE SHALL HAVE CLASS "R-B" BEDDING UNLESS OTHERWISE NOTED ON THE PLANS. SEE SHEET MD1 FOR BEDDING DETAILS.
 - ALL PRECAST PRODUCTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C-478.
 - ALL DRAINAGE STRUCTURES LOCATED WITHIN PAVEMENT MUST HAVE UNDER DRAIN AS SHOWN.

- PLYMOUTH NOTES**
- STORM SEWER THAT IS 12 INCHES AND LARGER SHALL BE C-76 R.C.P. WITH PREMIUM JOINTS.
 - THE REAR YARD STORM PIPE WHICH DOES NOT TRAVERSE PAVED AREAS MAY BE 8-INCH DIAMETER SOLID WALL, PVC OR ABS TRUSS OR PVC TRUSS PIPE.
 - BACKFILL FOR ALL REAR YARD STORM IS TO BE NATURAL BANK RUN SAND MDOT CL-II GRANULAR MATERIAL OR 3/4 INCH CRUSHED STONE TO A MINIMUM OF 12 INCHES OVER PIPE.
 - EVERY LOT IS TO HAVE DIRECT ACCESS TO A DRAINAGE STRUCTURE.
 - THE STORM SEWER WILL NOT BE CONSIDERED SUBSTANTIALLY COMPLETE UNTIL THE DETENTION POND HAS BEEN COMPLETED. THIS INCLUDES FINAL GRADING OF THE POND AND STABILIZATION OF THE SIDE SLOPES. THE SIDE SLOPES MUST BE STABILIZED BY SODDING OR BY PLACING A MULCH BLANKET PEGGED IN PLACE OVER SEED.
 - COVER FOR ON-LINE CATCH BASIN SHALL BE EJIW 1000 SERIES WITH TYPE N COVER OR NEENAH 2077-B WITH TYPE B COVER.
 - SUMP PUMP LEADS ARE TO BE A MINIMUM OF 3-INCH SCHEDULE 30 PVC.

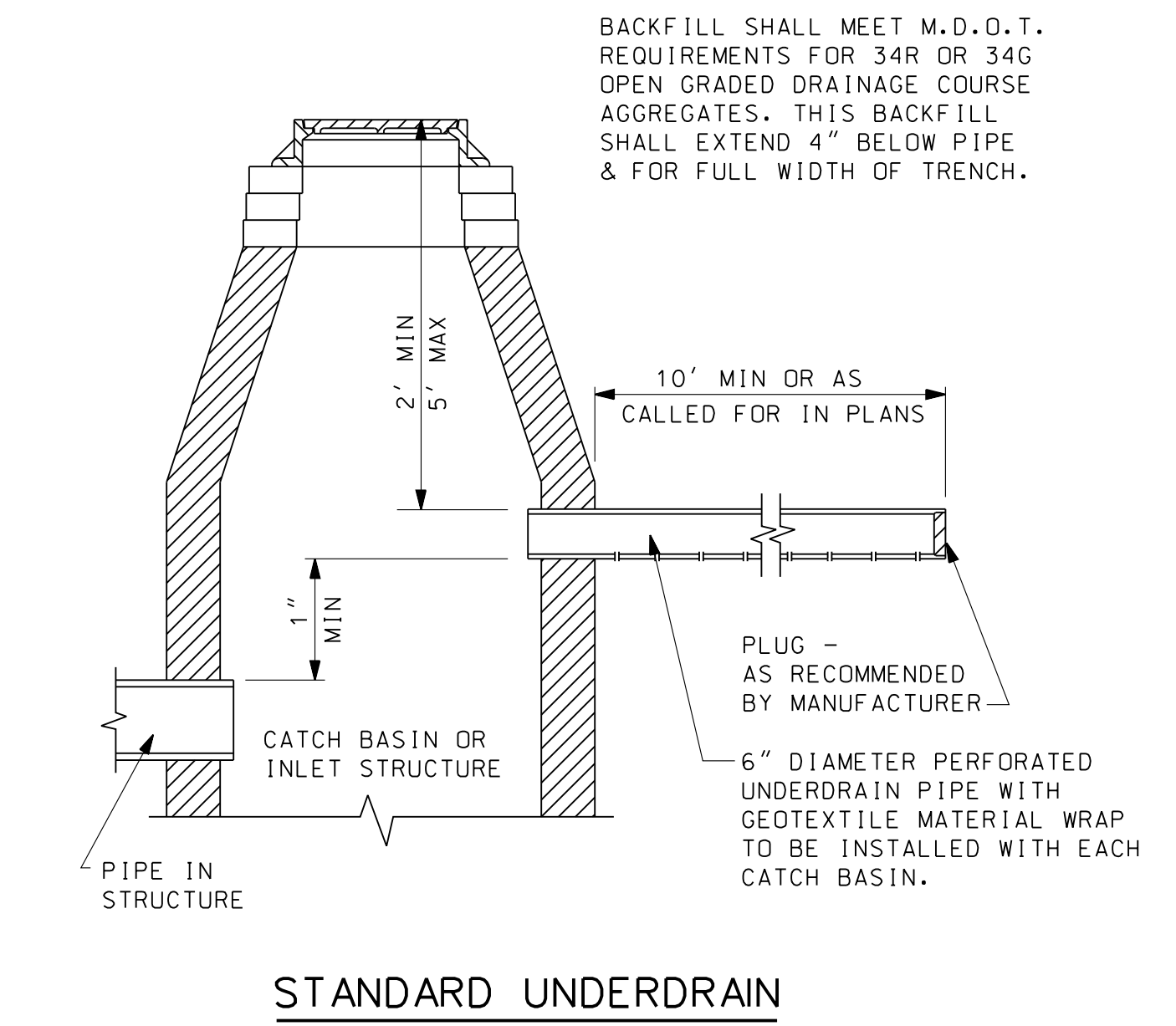


TYPICAL MANHOLE/ CATCH BASIN "D"
N.T.S.

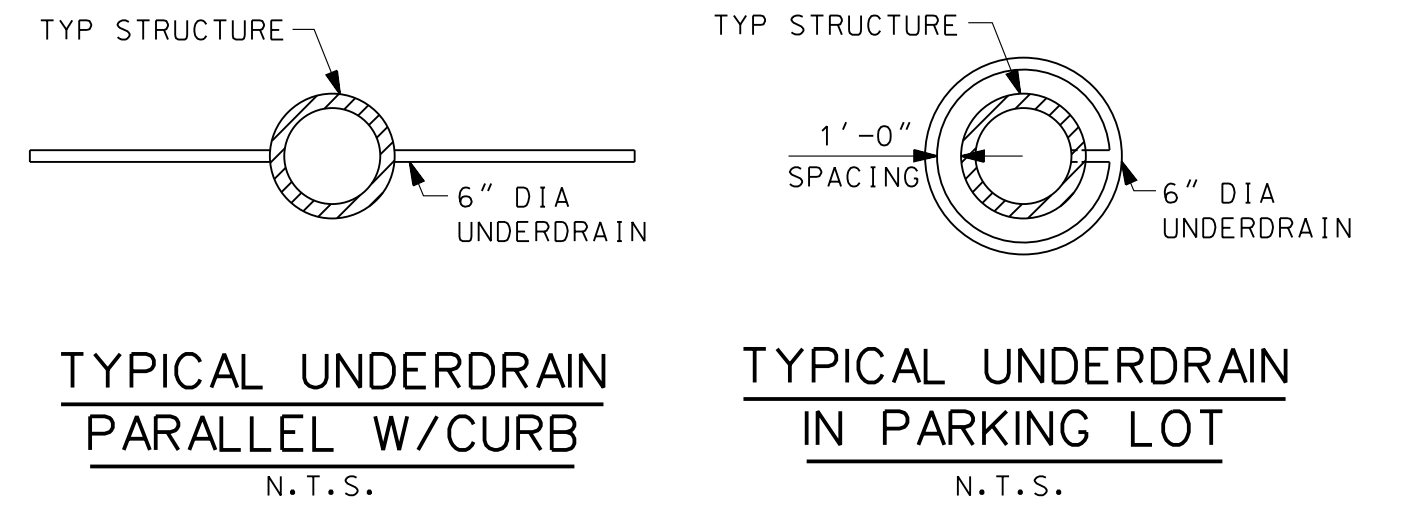


DIAMETER OF OUTLET PIPE	"X" INSIDE DIA.
12"-18"	4'-0" MIN
21"-30"	5'-0" MIN

STANDARD CATCH BASIN



STANDARD UNDERDRAIN



TYPICAL UNDERDRAIN PARALLEL W/CURB
N.T.S.

TYPICAL UNDERDRAIN IN PARKING LOT
N.T.S.

PROJECT MANAGER: [Name] FIELD BOOK INFORMATION: [Name] STANDARD STORM SEWER DETAILS (ST-1) OF 2

CITY OF PLYMOUTH
 201 SOUTH MAIN STREET
 PLYMOUTH, MICHIGAN 48170
 STANDARD STORM SEWER DETAILS (ST-1)

WADE TRIM
 26251 Northline Rd.
 PO Box 10
 Taylor, MI 48180
 www.wadefirm.com

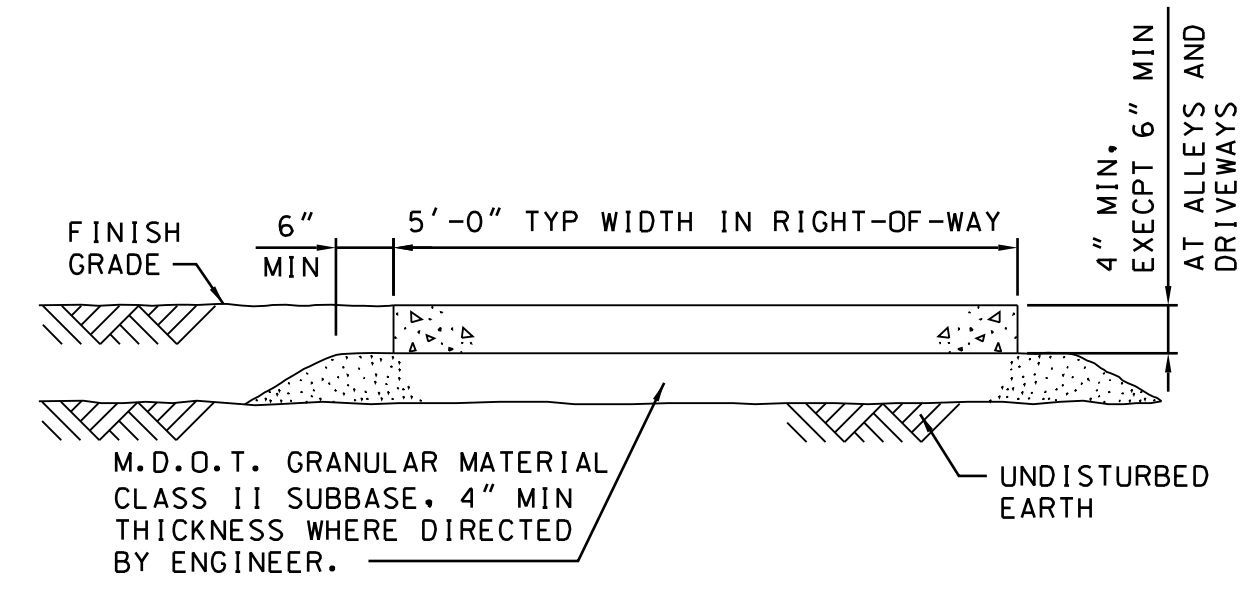
NOT VALID FOR CONSTRUCTION UNLESS SIGNED AND DATED

ISSUED FOR: DATE: BY:

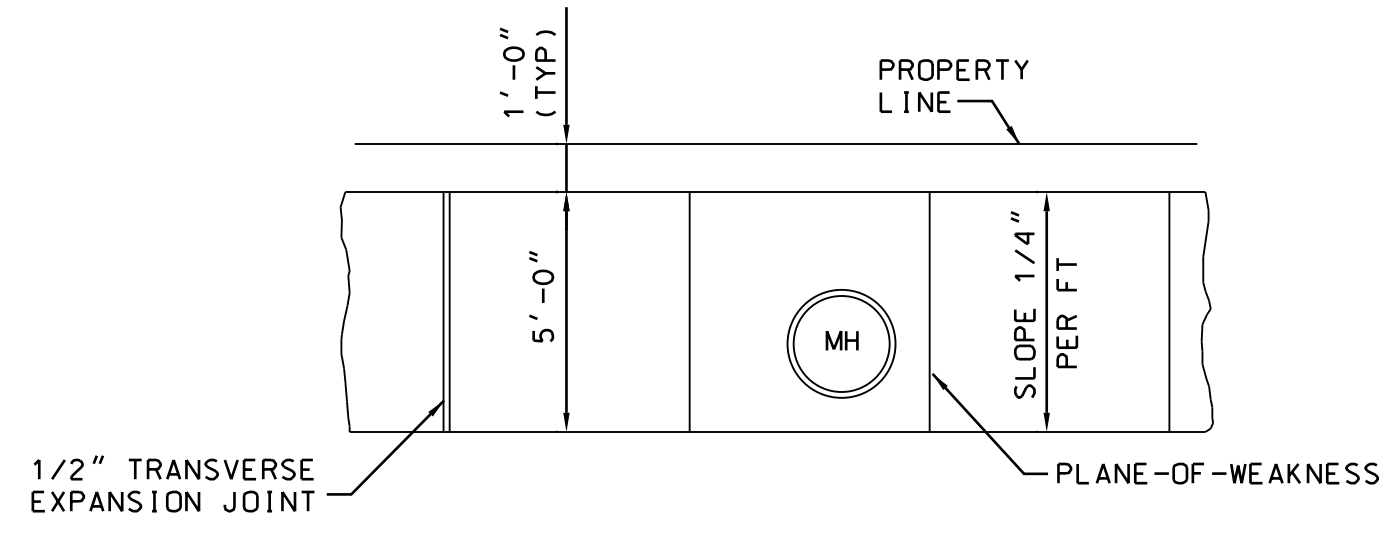
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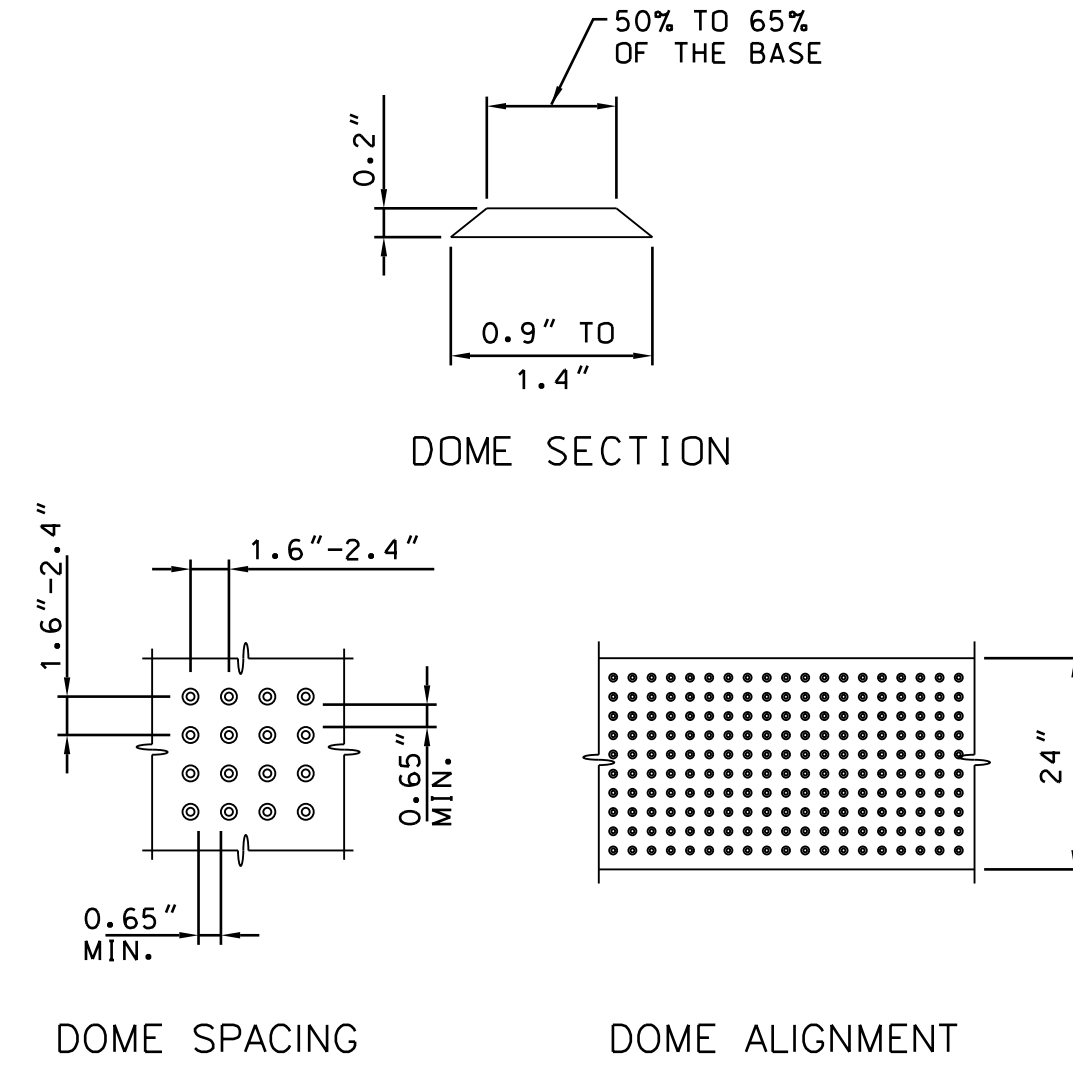
ST-1



TYPICAL SECTION
N.T.S.



TYPICAL PLAN IN RIGHT-OF-WAY
N.T.S.



DETECTABLE WARNING DETAILS
N.T.S.

NOTES:

- ONE-HALF INCH TRANSVERSE EXPANSION JOINTS SHALL BE PLACED THROUGH THE SIDEWALK AT UNIFORM INTERVALS OF NOT MORE THAN 50 FEET. ONE-HALF INCH EXPANSION JOINT SHALL ALSO BE PLACED BETWEEN THE SIDEWALK AND ABUTTING PARALLEL CURB, BUILDINGS, RIGID STRUCTURES, CONCRETE DRIVES AND CONCRETE DRIVE APPROACHES. ONE INCH EXPANSION JOINTS SHALL BE PLACED BETWEEN SIDEWALK RAMP AND BACK OF CURB.
- PLANE-OF-WEAKNESS JOINTS SHALL BE PLACED EVERY 5 FEET AND BE PRODUCED BY USE OF SLAB DIVISION FORMS EXTENDING TO THE FULL DEPTH OF CONCRETE OR BY CUTTING JOINTS IN THE CONCRETE, AFTER FLOATING, TO A DEPTH OF NOT LESS THAN ONE-FOURTH THE THICKNESS OF THE SIDEWALK. THE CUT JOINTS SHALL BE NOT LESS THAN 1/8 INCH NOR MORE THAN 1/4 INCH IN WIDTH AND SHALL BE FINISHED SMOOTH AND TRUE TO LINE.

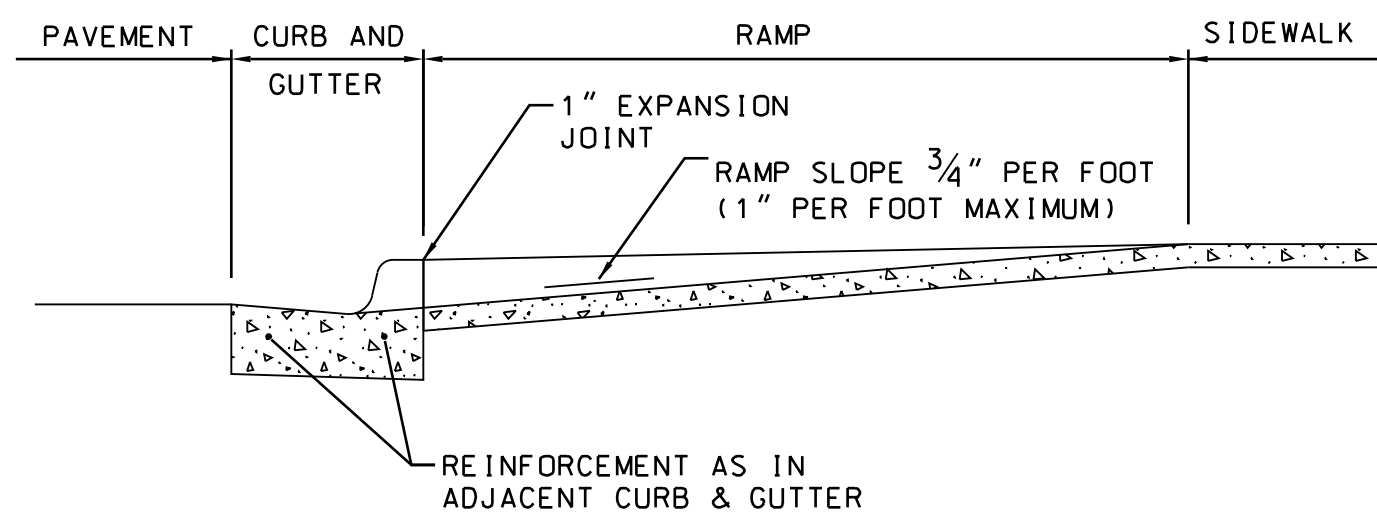
SIGN NOTES:

- SIGN PANEL SHALL BE 0.080 INCH THICK, ALUMINUM SHEET, TYPE 6061-T6 (MDOT TYPE III).
- HOLES IN SIGN POSTS SHALL BE CONTINUOUS 3/4\"/>

TYPE	BACKGROUND	LEGEND
A.	Reflectorized	Reflectorized
B.	Reflectorized	Non-Reflectorized
C.	Non-Reflectorized	Reflectorized

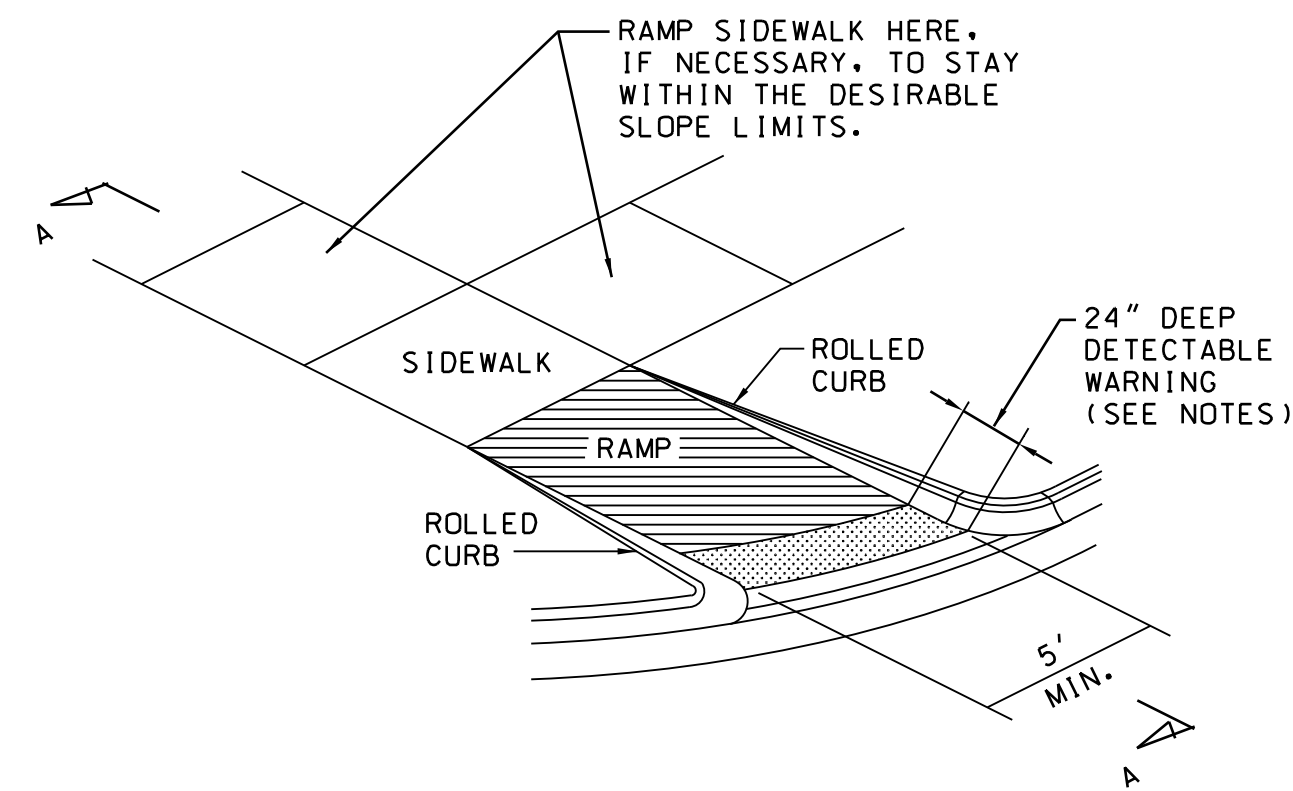
SIDEWALK RAMP NOTES:

- ALL SIDEWALK RAMP AS CALLED FOR ON THE PLANS SHALL HAVE DETECTABLE WARNINGS.
- DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH THE ADJACENT WALKING SURFACE.
- DETECTABLE WARNING SHALL BE MODULAR PLASTIC TILE, POLYMER CONCRETE TILE OR OTHER SIMILAR TILE EMBEDDED INTO THE FRESH CONCRETE, AS APPROVED BY THE ENGINEER; OR A STAMPED PATTERN IN THE FRESH CONCRETE WITH INTEGRAL COLOR (NOT SHAKE-ON COLOR). ADHESIVE APPLIED DETECTABLE WARNINGS ARE NOT ACCEPTABLE.
- ALL DETECTABLE WARNINGS SHALL HAVE SKID RESISTANT SURFACE.
- SYSTEM AND COLOR SHALL BE APPROVED BY THE ENGINEER PRIOR TO BEGINNING WORK.



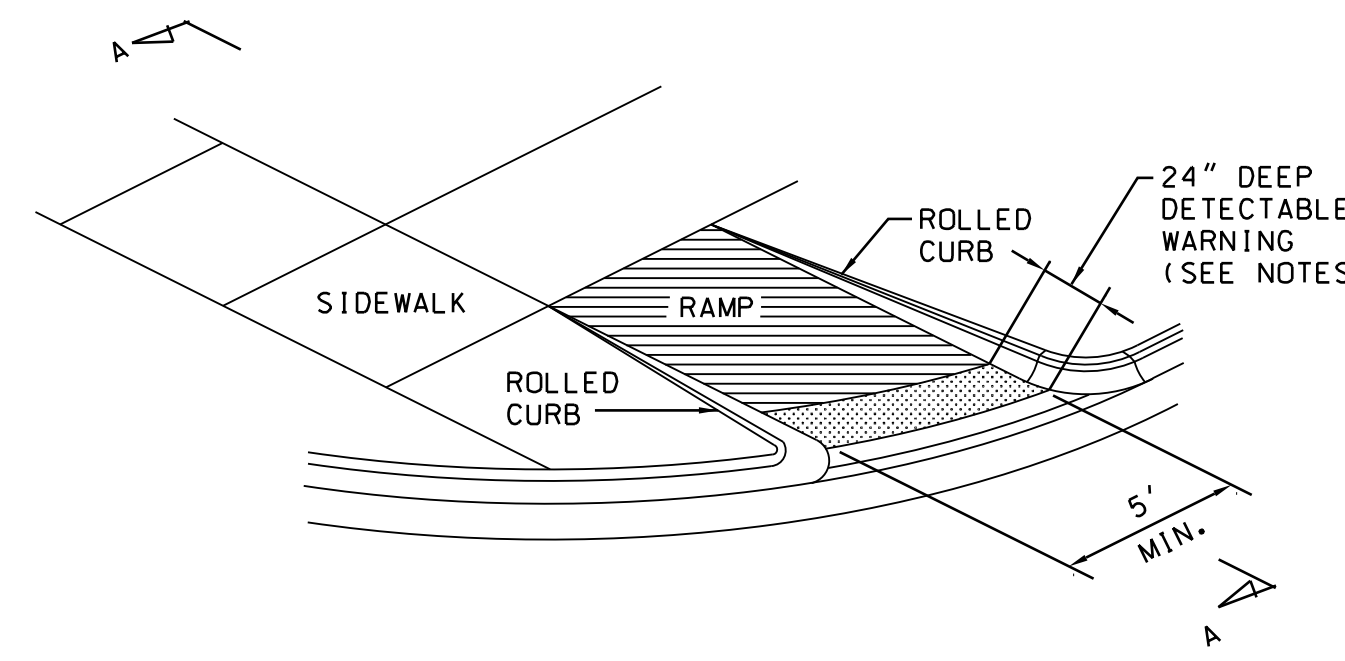
SECTION A-A
(TYPICAL ALL RAMP DETAILS)

TYPICAL PLAN IN RIGHT-OF-WAY
N.T.S.



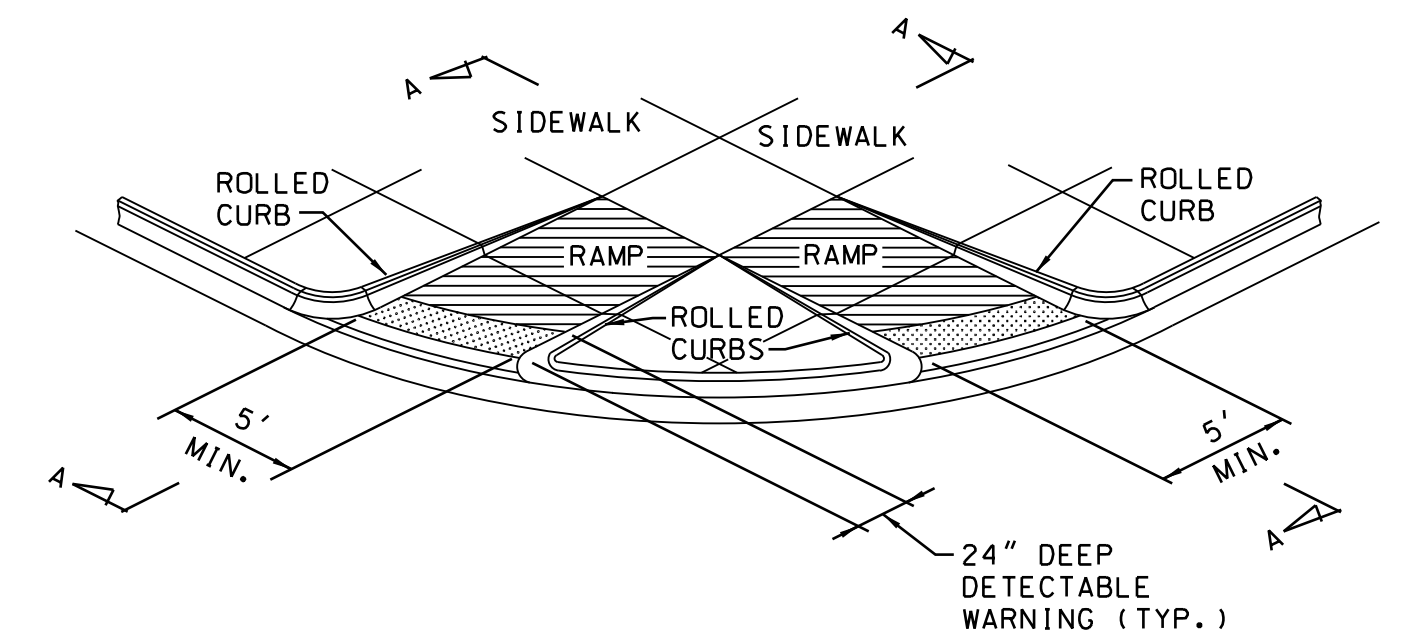
SIDEWALK RAMP TYPE 1

TYPICAL PLAN IN RIGHT-OF-WAY
N.T.S.



SIDEWALK RAMP TYPE 2

SIDEWALK RAMP DETAILS
N.T.S.



(TWO RAMP ARE SHOWN)

SIDEWALK RAMP TYPE 4
N.T.S.

PROJECT MANAGER: FIELD BOOK INFORMATION: PROJECT: WASTE TRIM STANDARD SIDEWALK DETAILS CITY OF PLYMOUTH, MICHIGAN 48170 DATE: 2/13/2024 BY: mlomasgn

REV.	DATE	DESCRIPTION	BY

NOT VALID FOR CONSTRUCTION UNLESS SIGNED AND DATED:

26251 Northline Rd.
PO Box 10
Taylor, MI 48180
www.wadeftrim.com

CITY OF PLYMOUTH
201 SOUTH MAIN STREET
PLYMOUTH, MICHIGAN 48170
STANDARD SIDEWALK DETAILS (SW-1)

ISSUED FOR:	DATE:	BY:
JOB NO.		
SHEET	SW-1	

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Carlisle | Wortman
ASSOCIATES, INC.

117 NORTH FIRST STREET SUITE 70 ANN ARBOR, MI 48104 734.662.2200 734.662.1935 FAX

Date: June 6, 2024

**Planned Unit Development
For
City of Plymouth, Michigan**

GENERAL INFORMATION

Applicant:	Donofrio Wil Cox, LLC 1156 Beech St. Plymouth, MI 48170
Project Name:	Wilcox Mill Restoration PUD
Plan Date:	January 8, 2024
Location:	230 Wilcox Road, Plymouth, MI
Zoning:	R-1, Single-Family Residential District
Action Requested:	Preliminary PUD Approval

PROJECT DESCRIPTION

The applicant has submitted a Planned Unit Development (PUD) application for land that was formally owned by Wayne County. The property is located adjacent to Edward Hines Drive and the recreational facilities provided by Wayne County Parks. The project proposes to restore the existing, 2-story, historic structure (approx. 4,800 s.f.) into a first floor coffee shop, with a potential retail use on the lower level aimed at enhancing the Park experience. An existing, 16-space parking lot at the rear of the property will also be renovated, and a new, 38-space parking lot will be constructed, with barrier-free access from this lot to the first floor of the building.

An aerial of the proposed project site is shown on the next page.



Source: Near Map (Capture April 8, 2024)



Source: Google Maps (Capture August, 2019) – Looking West from Hines Drive.

In 2019-2020, the Planning Commission discussed a PUD proposal that would renovate the building into space for an art foundation, an art studio for the owner, and two residential units on a new third floor.

The Planning Commission approved the Preliminary PUD. However, the property owner didn't pursue the project any further. The current proposal is from a new property owner, with a new concept.

PUD PROCESS AND PLAN REQUIREMENTS

According to Section 78-314 of the Zoning Ordinance and the prescribed PUD procedures for review, the applicant attended a pre-application conference with City staff in January, 2021. A formal application has been submitted and includes the transmittal of preliminary PUD plans/site plans.

A preliminary site plan submission that accompanies a PUD request includes the following requirements, which need to be provided:

1. A written explanation of how the project meets the PUD eligibility criteria (Sec. 78-311(c)).
2. Zoning of applicant's property and all abutting properties and of properties across any public or private street from the PUD site added to the plans.
3. Has Wayne County restricted the building renovations or use as part of the purchase agreement? The applicant needs to provide a copy of the purchase agreement to the City to document any conditions/restrictions. Also, written information about how the County will be involved, if at all, in assessing whether the purchase agreement has been accurately implemented or not also needs to be provided.

Items to be Addressed: 1) Provide written explanation of how the project meets the PUD eligibility criteria in Sec. 78-311(c). 2) Add existing zoning of subject site and adjacent properties to site plan. 3) Submit purchase agreement with County to City to document any conditions/restrictions to redevelopment of the building or site. 4) Provide written information about how, if at all, the County will be involved in assessing whether the purchase agreement has been accurately implemented.

PUD ELIGIBILITY

Section 78-311(c) of the City of Plymouth Zoning Ordinance establishes PUD criteria which determine the overall eligibility for a Planned Unit Development. These criteria are provided below.

(1) Grant of the planned unit development will result in one (1) of the following:

- a. A recognizable and material benefit to the ultimate users of the project and to the community, where such benefit would otherwise be unfeasible or unlikely to be achieved without application of the planned unit development regulations;**

CWA Comment: This site has an unusual shape, and its location on a corner increases the non-buildable area of the property. The location of existing utilities, tail race, and topography also make this site challenging to develop. However, if it were developed as zoned (single-family residential use), we estimate that the site could possibly accommodate approximately 12-15 clustered dwelling units west of the tail race. But, as mentioned above, we don't know the conditions/restrictions placed upon the property by Wayne County (if any), which could further restrict development as single-family residential.

The submission is proposing to renovate the existing historic structure on site, renovate an existing parking lot, and build a new lot and barrier-free access to the first floor of the building. While we consider renovation of a historic structure a “material benefit,” the use of the renovated structure also needs to be considered.

The applicant explains that their plans for use of the renovated historic structure include a coffee shop, and some type of retail facility on the lower level that is related to the recreational facilities in the River Rouge Parkway park. None of these uses would be permitted under the current residential zoning, as they are considered “commercial” uses in the Zoning Ordinance.

Typically, land uses in a PUD are limited to uses that are allowed in the underlying zoning district. However, Sec. 78-311(b) of the Zoning Ordinance states the following:

“(b) Any land use authorized in this article may be included in a planned unit development, subject to adequate public health, safety, and welfare protection mechanisms being designed into the development to ensure the compatibility of varied land uses both within and outside the development.”

Therefore, it would be possible to consider land uses other than those permitted in the underlying zoning as long as there are adequate protections for the neighbors. To better understand the proposal, the applicant should provide the following information:

1. We assume from the description provided that the proposed occupants for both units will incorporate “retail sales.” The applicant should confirm this.
2. Has the applicant secured tenants for the building, or will they be operating the proposed businesses themselves? If so, information about the tenants should be provided, including:
 - a. Type of business
 - b. Maximum occupancy of each unit
 - c. Proposed hours of operation for both inside the building, as well as use of the outdoor patios
 - d. Will any outdoor music be offered, or outdoor speakers be installed?
3. The other historic building along Hines Drive is currently being used as an office. Are other types of businesses, such as office uses, contemplated for either unit?

Once this information has been provided, the uses can be evaluated.

In any event, we recommend that if the Planning Commission supports the project, that a specific description of permitted building/site uses (i.e., uses that support recreational activities in the River Rouge Parkway park, including...) be added as a condition in the PUD Agreement.

- b. Long-term protection and preservation of natural resources and natural features of a significant quantity and/or quality, where such benefit would otherwise be unfeasible***

or unlikely to be achieved without application of the planned unit development regulations;

CWA Comment. The proposed development will preserve the existing tail race, and will not impact the existing wetland on the southern boundary of the property. The applicant should indicate if there is an easement over the existing tail race, as it serves the entire Rouge River, which is under the jurisdiction of the State.

The project will remove vegetation along Wilcox to construct the barrier-free walkway to the parking lot, but the use of retaining walls will help to limit the clearing and grading needed to implement this change. The existing tree survey shows one significant tree (#84, 25" Hickory) in the vicinity of the walkway. To meet this criterion, the grading plan should be designed to be well outside of the dripline of this tree.

The Site Development Plan shows the location of the floodplain boundaries on the site. A portion of the new parking lot, and the barrier-free walkway will be located within the floodplain. This sheet also notes the following: *“Currently the floodplain location and elevation are being evaluated to remove the area of development (building, parking lots and rain gardens) from the floodplain. Once FEMA has determined the new floodplain, these plans will be updated to show new floodplain information.”* The project is not locating any new buildings within the floodplain. The applicant should describe the status of this map amendment.

To ensure preservation of the tail race and remaining natural features on site, we recommend that the plans show a “conservation easement” over the areas they intend to preserve. This will ensure that the site will maintain is “park-like” character, and be consistent with the Rouge River Parkway into the future.

c. Long-term protection of historic structures or significant architecture worthy of historic preservation; or

CWA Comment. As part of this project, the applicant is proposing to restore the existing building on site, which was originally designed by the famous architect Albert Kahn for Henry Ford. The applicant states that part of this restoration will be saving and restoring the original steel sash window frames and the Mill Road-facing façade (assuming they mean “Wilcox Road”), bringing back the original look. We consider this work to meet this criterion. However, additional information on the proposed uses, and scope of those uses, needs to be provided.

d. A nonconforming use shall, to a material extent, be rendered more conforming, or less offensive, to the zoning district in which it is situated.

CWA Comment: This site was used as part of the Wayne County park system, which is a permitted use in the R-1 zoning district. The proposal is suggesting changing the site’s use to uses that are not currently permitted in the R-1 zoning district. Therefore, the proposal will not make this site less non-conforming. However, renovating the building and site will make it more aesthetically pleasing to the neighborhood.

(2) The proposed type and density of use shall not result in an unreasonable increase in the need for or burden upon public services, facilities, roads and utilities.

CWA Comment: The site plan shows utility structures for a sanitary sewer, but no water main. No new utilities are proposed, except for stormwater management facilities. Whether the proposed utilities meet this criterion needs to be confirmed by the City’s Engineer and an opinion provided at this stage of the project review.

Regarding traffic, we researched trip generation tables for a “coffee shop” and “sporting goods superstore.” Note that the trip generation tables don’t have a category for a small-scale sporting goods store. However, traffic accessing the sporting goods superstore category is calculated per 1,000 s.f., which can be applied to this proposal. It’s also likely that people would be accessing this use on their bicycle, given that Hines Drive is often used by cyclists.

Table 1. Estimate Traffic Generation – Peak Hours

Use	Calculation*	Estimated # of Vehicle Trips (Peak Hour)
Coffee Shop (NO drive thru)	96.43 trips/1,000 s.f. (AM Peak Hour - Weekday) 56.50 trips/1,000 s.f. (Peak Hour – Weekend)	Weekday Peak: 212 trips (96.43 x (2,200 s.f./1,000)) Weekend Peak: 124 trips (56.5 x (2,200 s.f./1,000))
Sporting Goods Store	2.58 trips/1,000 s.f. (PM Peak Hour - Weekday) 3.99 trips / 1,000 s.f. (Peak Hour - Weekend)	Weekday Peak: 6 trips (2.58 x (2,200 s.f. / 1,000)) Weekend Peak: 9 trips (3.99 x (2,200 s.f./1,000))
TOTAL		Weekday Peak: 218 trips Weekend Peak: 133 trips

*Note that the trip generation tables use “gross” square feet.

The table above includes the times when the proposed uses generate the most traffic. “Peak hour” indicates the hour with the highest volume of vehicle trips entering and existing a site. This means that the number of trips represents 50% entering the site and 50% existing the site, which illustrates the impacts this traffic would have on the adjacent street.

The Annual Average Daily Traffic count (per [SEMCOG’s Traffic Volume Map](#)) indicates that Wilcox Road experiences approximately 5,200 vehicle trips per day, and Edward Hines Drive experiences approximately 4,700 vehicle trips per day. Even though Wilcox is only two lanes, this road is identified as a “Major Collector” in the Plymouth Master Plan, indicating that this road funnels traffic from residential areas to arterial roads. Given the existing traffic on the adjacent streets, we don’t think the traffic generated at the peak hours by the proposed uses will create traffic problems.

- (3) *The proposed planned unit development shall be consistent with the public health, safety and welfare of the City.***

CWA Comment: The previous proposal showed a “monitoring well” near the back of the building (north side of trail race). This well isn’t shown on the current survey, but may have been overgrown with vegetation. The location and purpose of the monitoring well should be investigated, shown on the plans, and purposed described.

The previous PUD discussed how remediation of contaminants on site would be remediated. However, this set of plans doesn’t discuss any existing contamination that will need to be addressed before inviting the public onto the property. If any contamination exists on the site, a written description should be provided. Also, if any clean-up has already been conducted, a written description of the clean-up activities should also be provided.

- (4) *The proposed planned unit development shall not result in an unreasonable negative environmental impact or loss of a historic structure on the subject site or surrounding land.***

CWA Comment: Regarding unreasonable negative environmental impact, it doesn’t appear with the information provided that the site’s natural features will be negatively impacted. However, additional information as discussed above needs to be provided.

Regarding loss of a historic structure, the applicant is proposing to renovate and re-use the existing historic structure.

- (5) *The proposed planned unit development shall not result in an unreasonable negative economic impact upon surrounding properties.***

CWA Comment: Renovation of this building and site will, in our opinion, benefit the economics of the surrounding area as long as the uses are appropriate, and the level of activity on this site is consistent with the character of the area.

- (6) *The proposed planned unit development shall be under single ownership and/or control such that there is a single person, corporation, or partnership having responsibility for completing the project in conformity with this article.***

CWA Comment: Per the application form, the project is under ownership of Donofrio Wil Cox, LLC, who will be responsible for managing the project in conformity with the development agreement.

- (7) *The proposed planned unit development shall be consistent with the goals and policies of the City Master Plan.***

CWA Comment: The City’s 2018 Master Plan designates the future land use of the property as “Parks and Open Space.” This designation is described as follows:

“The Parks and Open Space land use category includes existing parkland and open/green spaces, as well as proposed parks that are identified in the City’s Recreation Master Plan. In addition to City-owned parks, this category also includes the County-owned River Rouge Parkway. This land use category is intended to protect parklands and open space from future

development that does not consider the public benefit of retaining such land. In areas where open green space cannot be retained, Single-Family Residential: Low Density development is preferred.”

The proposal intends to renovate and reuse the existing historical building, and improve/expand the parking on the property. The remainder will be used for stormwater management, maintenance of the tail race, and preserved open space. In our opinion, uses that support the park uses along Hines Drive would be consistent with the Master Plan, as long as their operation did not disrupt the adjacent residential uses.

We recommend that if the Planning Commission supports the proposal, that a condition be added to the PUD Agreement to deed restrict future development of the site, prohibit expanded retail/commercial buildings/development of the property, and preserve the open space as identified on the plans in an effort to preserve the recreational character of Hines Drive and the River Rouge Parkway park.

- (8) *The proposed use or uses shall be of such location, size, and character as to be in harmony with the zoning district in which it is situated, and shall not be detrimental to the adjoining zoning districts.***

CWA Comment: Because the existing building is being retained, we consider the historic building to be in harmony with, and not detrimental to, adjoining residential zoning districts. As mentioned above, the PUD Agreement should specifically list the uses that can occupy the building over time so that future uses are of types that will not negatively impact the residential neighborhood.

- (9) *A demonstration that the PUD is not proposed in an attempt by the applicant to circumvent the strict application of zoning standards.***

CWA Comment: The proposal is not possible under the current zoning. Therefore, either the site needs to be rezoned to a specific district, or the applicant needs to request a Planned Unit Development that would allow flexibility from ordinance standards.

Historically-accurate renovations to a building can be quite expensive. Also, the location of the existing building doesn't lend itself to being a single-family home. Also, if the property were developed as zoned, it's possible that renovation of the historic building wouldn't happen, or the building would be demolished. We don't consider the proposal an attempt to circumvent the ordinance requirements, as the restoration of this building will benefit the community. The allowed uses will need to be clearly defined to ensure they don't negatively impact the residential neighbors.

In summary, the applicant needs to provide additional information to fully evaluate whether the proposal meets the PUD criteria.

Items to be Addressed:

- 1) *Applicant to confirm that both units are proposed to be occupied by “retail sales” uses.*
- 2) *Provide a fuller description of proposed uses including:*
 - a. *Type of business*
 - b. *Maximum occupancy of each unit*

- c. *Proposed hours of operation for both inside the building, as well as use of the outdoor patios*
- d. *Will any outdoor music be offered, or outdoor speakers be installed?*
- 3. *Applicant to describe if other types of businesses, such as office uses, contemplated for either unit?*
- 4. *Grading Plan submitted at Final show that soil disturbance be well outside of the dripline of Tree #84, 25" Hickory.*
- 5. *Locate easement of tail race on site plan, if any.*
- 6. *Applicant to describe the status of the FEMA map amendment.*
- 7. *Plans show a "conservation easement" over the natural features/areas they intend to preserve.*
- 8. *Whether the proposed utilities meet the PUD criterion needs to be confirmed by the City's Engineer and an opinion provided at this stage of the project review.*
- 9. *The location and purpose of the monitoring well should be investigated, shown on the plans, and purposed described.*
- 10. *If any contamination exists on the site, a written description should be provided. Also, if any clean-up has already been conducted, a written description of the clean-up activities should also be provided.*

We also recommend that, if the Planning Commission supports the project, that the PUD Agreement include the following conditions:

- 1. *A specific description of permitted building/site uses (i.e., uses that support recreational activities in the River Rouge Parkway park, including...).*
- 2. *Deed restrictions on future development of the site, prohibitions on expanded retail/commercial buildings/development of the property, and preservation of the open space as identified on the plans.*

SCHEDULE OF REGULATIONS

Section 78-313 states that the schedule of regulations for each respective land use must be met, unless the Planning Commission and City Commission approve deviations that advance the objectives of the Zoning Ordinance.

This proposal includes retail uses in one building, which is similar to a shopping center that has various units for different retail uses. Also, because the building is located on highly traveled streets, we have applied the General Business District provisions in the schedule of regulations.

Table 2. General Business Schedule of Regulations Requirements

		GB Required	Proposed (Existing Building)
Minimum Lot Size		--	4.275 acres (186,230 s.f.)
Minimum Lot Width		--	273 feet (Wilcox) 589 feet (Hines Drive)
Height of building		2 stories / 30 feet	2 stories / ?? feet
Minimum yard setback	Front (Wilcox)	--	20 feet
	Front (Hines Drive)	--	117 feet
	Sides Least one	10 least	105 feet (west)
	Rear	10 feet	383 feet (South)
Maximum Lot Coverage		--	1.1%

	GB Required	Proposed (Existing Building)
Maximum Floor Area Ratio	--	0.023

Height: The height of the existing building should be provided. Note that even if the height of the building was taller than permitted in the district, it would be considered a legally existing non-conformity, and not a PUD deviation.

Items to be Addressed: Provide height of existing building.

PARKING, LOADING

The table below shows the required parking, as well as the parking proposed on the site plan:

Table 3. Parking Requirements

	Parking Required – Base Uses	Parking Provided
Mixed-Use Building:		
Lower Level Retail	1 space per 200 s.f. usable floor area (1,975 s.f. / 200 = 10 spaces)	54 spaces (See Below)
Upper Level Coffee Shop (Considered a “restaurant” in ZO)	1 space per 75 s.f. of usable floor area (1,975 s.f. / 75 = 27 spaces)	
TOTAL	37 spaces	

Number of Parking Spaces

The number of parking spaces proposed will accommodate two restaurants (or one restaurant occupying the entire building). Restaurants are not considered “retail” uses under the Zoning Ordinance. They are more intense than a general retail use, or a “snack shop,” and are treated differently in the ordinance.

The project application form states that the intended uses are “general retail uses.” Under the Zoning Ordinance, this would not include a full-scale, sit-down restaurant(s) or bar. We also don’t think a sit-down restaurant or bar to be consistent with the intent of the “park” designation in the Master Plan. A coffee shop, ice cream store, or other “snack shop” would serve the recreational users of the Rouge River Parkway, but attracting users from a wider vicinity (such as a destination “restaurant”) arriving by car would not, in our opinion, be consistent with preserving the “park” atmosphere of this site. This use would also be disturbing to the residential neighbors, as a restaurant/bar would be open later into the evenings, and any outdoor dining/patio use would create noise that would disturb the neighborhood. The Planning Commission should discuss the concept the applicant has for this project to refine the uses the Commissioner’s consider consistent with the Master Plan.

Screening of Parking Lots

Section 78-203 requires a 10-foot wide landscape strip to screen all parking lots that are visible from a public right-of-way. The proposed landscape plan shows “meadow lawn” along Hines Drive, which would not provide the required screen. An adequate vegetated screen needs to be provided.

Parking Lot/Space Dimensions

The dimensions of the proposed parking spaces do not meet ordinance requirements. The length of a space must be at least 20-feet long.

- If a seven (7)-foot wide sidewalk abuts a parking space, then 2-feet of the walk may be counted toward the parking space length. The walk adjacent to both parking lots is only five-feet wide. Therefore, the parking space length needs to be lengthened to 20-feet, or the sidewalks widened to a minimum of 7-feet wide.
- The parking spaces that don’t abut a sidewalk need to be lengthened to 20-feet-long.

Note that the Planning Commission could consider the parking space length (18-feet) as a deviation. However, because the sidewalks must accommodate ADA standards, the sidewalks must be widened to 7-feet.

Loading Space

The plans show a 10’ x 40’ loading/unloading space in the easterly lot next to the building. The ordinance requires a 10’ x 50’ loading space for a building of this size. The plans should describe the types/sizes of trucks expected to make deliveries/pick-ups to evaluate whether the proposed loading/unloading zone is sufficient. Also, turning radii of the proposed delivery trucks should be shown on the plans to ensure that a delivery truck can successfully use the loading/unloading zone as proposed.

Items to be Addressed: 1) Planning Commission to discuss proposed uses of site with applicant to clarify their vision for uses of the property (i.e., “retail” vs. “restaurant.”) 2) Planning Commission to determine what uses are appropriate in this location, ensuring that the proposed uses won’t disturb the adjacent residential neighbors. 3) Provide adequate parking lot screen along Hines Drive. 4) Increase length of parking spaces, or Planning Commission consider a PUD deviation. 5) Increase width of sidewalks abutting parking lot to a minimum of 7-feet wide. 6) Applicant to provide types/sizes of proposed delivery trucks on plans to evaluate proposed loading/unloading space size and orientation. 7) Applicant to show turning movements of delivery trucks on plans to ensure loading/unloading space is usable.

CIRCULATION

This site has two existing access points on the lower level; one to the existing parking lot along Hines Drive, and the other to a graveled area south of the first. Also, the upper level has a paved shoulder in the right-of-way in front of the existing building along Wilcox Road. On the subject site, the plans show a knee wall and bollards in front of a patio made of brick pavers. (Note that the engineered site plan and landscape plan are slightly different on this portion of the site.)

We have no comments on the functionality of the existing driveways off of Hines Drive. However, it may be necessary to receive Wayne County Road Commission approval to use these existing drives for the proposed new uses.

Regarding the area in front of the existing building along Wilcox, the Landscape Plan shows that there will be about 8-feet of pavement along the road edge where a car can pull up to the knee wall/bollards/front

patio. A person will not have much room to get out of the traffic, and be able to open the vehicle doors to exit the care. This could be a tricky maneuver during high traffic times of day. We would recommend that the knee wall be shifted at least two (2) feet toward the building to allow people to park and open their doors without obstructing traffic along Wilcox Rd.

Items to be Addressed: 1) Need for Wayne County Road Commission approval of new use of existing driveways off of Hines Drive. 2) Recommend that knee wall along Wilcox Rd. be shifted at least 2-feet toward the building to allow at least 10-foot-wide drop-off/pick-up area within the road right-of-way.

SIDEWALKS

The existing conditions along Wilcox and Hines Drive include a paved shoulder (see photo of Wilcox right-of-way below, with subject site in the background). No independent pedestrian facilities (such as a sidewalk or safety path) are provided along either frontage of the subject site, or the adjacent properties.



The Master Plan does not call for any pedestrian facilities along Wilcox Rd.

A ramped sidewalk is proposed between the upper level and the new parking lot in lower level of the site. This walkway will provide a barrier-free access from the upper level of the building to the lower level of the building and parking lot. Whether the ramp meets the requirements of the American with Disabilities Act will need to be assessed. Outdoor stairs are also provided between the upper and lower levels of the site for those not needing the ramp.

Will the building contain an elevator? Will stairs be provided inside the building between the two levels?

Items to be Addressed: 1) Outdoor pedestrian ramp compatibility with ADA requirements needs to be assessed. 2) Will building contain an elevator or interior stairs?

LANDSCAPING/LIGHTING

Because this is a Preliminary Site Plan/PUD Plan review, we haven't evaluated the proposed landscaping. A lighting plan, meeting the requirements of Sec. 78-204, *Exterior Lighting*, will also need to be submitted at Final review. We will evaluate both at the Final Site Plan/PUD Plan stage.

Items to be Addressed: *Landscaping/lighting evaluated as part of the Final Site Plan/PUD stage.*

PUD AGREEMENT / PHASING

A PUD Agreement will need to be developed prior to final approval. The agreement will specify performance guarantees and phasing. It will also most likely include the conditions/restrictions placed upon the property by Wayne County (if any) and the City.

Items to be Addressed: *Develop PUD Agreement with performance guarantees for public benefits.*

ARCHITECTURAL ELEVATIONS AND FLOOR PLANS

The applicant has provided an architectural rendering of the proposed renovated building. The submitted narrative states that the renovations will include restoration of the existing the original steel sash window frames, which is an important component of the historic integrity of this building. If the Planning Commission supports this project, and because the main "public benefit" is restoration of the historic building, we recommend the following conditions be included in the PUD Agreement:

- The historic restoration will be accomplished in compliance with the Secretary of Interior Standards for Rehabilitation of Historic Structures, and that an architectural historian that meets the minimum qualifications published in the Code of Federal Regulations (36 CFR Part 61) will oversee the work.
- The historic restoration will include restoration of the original steel sash window frames, and that the window frames will not be replaced with modern window frames. (Note: Replacement of the glass with modern glass is acceptable.)
- The historic restoration will be done in compliance with any agreement with Wayne County, if historic restoration of the building is included in the purchase agreement for the property.

No floor plans have been provided, but should be.

Items to be Addressed: *1. Recommend following conditions if Planning Commission supports the project:*


- a. *The historic restoration will be accomplished in compliance with the Secretary of Interior Standards for Rehabilitation of Historic Structures, and the work is overseen by an architectural historian that meets the minimum qualifications published in the Code of Federal Regulations (36 CFR Part 61).*
 - b. *The historic restoration will include restoration of the original steel sash window frames, and that the window frames will not be replaced with modern window frames. (Note: Replacement of the glass with modern glass is acceptable.)*
 - c. *The historic restoration will be done in compliance with any agreement with Wayne County, if historic restoration of the building is included in the purchase agreement for the property.*
- 2. Provide floor plans.*

RECOMMENDATIONS

We support restoration of the historic building on this site, and the general layout of the other site amenities. However, we have many questions about the proposed uses, and the scope of these uses, and their impact on the neighborhood. We recommend that the applicant revise the plans and provide the information listed in this review before the Planning commission considers PUD eligibility. (Note that we have listed the recommended conditions in this review under the PUD Agreement topic heading below).

- A. PUD Plan Requirements:** 1) Provide written explanation of how the project meets the PUD eligibility criteria in Sec. 78-311(c). 2) Add existing zoning of subject site and adjacent properties to site plan. 3) Submit purchase agreement with County to City to document any conditions/restrictions to redevelopment of the building or site. 4) Provide written information about how, if at all, the County will be involved in assessing whether the purchase agreement has been accurately implemented.
- B. PUD Eligibility.** 1) Applicant to confirm that both units are proposed to be occupied by “retail sales” uses.
- 2) Provide a fuller description of proposed uses including:
- a. Type of business
 - b. Maximum occupancy of each unit
 - c. Proposed hours of operation for both inside the building, as well as use of the outdoor patios
 - d. Will any outdoor music be offered, or outdoor speakers be installed?
- 3) Applicant to describe if other types of businesses, such as office uses, contemplated for either unit?
- 4) Grading Plan submitted at Final show that soil disturbance be well outside of the dripline of Tree #84, 25” Hickory.
- 5) Locate easement of tail race on site plan, if any.
- 6) Applicant to describe the status of the FEMA map amendment.
- 7) Plans show a “conservation easement” over the natural features/areas they intend to preserve.
- 8) Whether the proposed utilities meet the PUD criterion needs to be confirmed by the City’s Engineer and an opinion provided at this stage of the project review.
- 9) The location and purpose of the monitoring well should be investigated, shown on the plans, and purposed described.
- 10) If any contamination exists on the site, a written description should be provided. Also, if any clean-up has already been conducted, a written description of the clean-up activities should also be provided.
- C. Schedule of Regulations.** Provide height of existing building.
- D. Parking/Loading.** 1) Planning Commission to discuss proposed uses of site with applicant to clarify their vision for uses of the property (i.e., “retail” vs. “restaurant.”) 2) Planning Commission to determine what uses are appropriate in this location, ensuring that the proposed uses won’t disturb the adjacent residential neighbors. 3) Provide adequate parking lot screen along Hines Drive. 4) Increase length of parking spaces, or Planning Commission consider a PUD deviation. 5) Increase width of sidewalks abutting parking lot to a minimum of 7-feet wide. 6) Applicant to provide types/sizes of proposed delivery trucks on plans to evaluate proposed loading/unloading space size and orientation. 7) Applicant to show turning movements of delivery trucks on plans to ensure loading/unloading space is usable.

- E. **Circulation.** 1) Need for Wayne County Road Commission approval of new use of existing driveways off of Hines Drive. 2) Recommend that knee wall along Wilcox Rd. be shifted at least 2-feet toward the building to allow at least 10-feet-wide drop-off/pick-up area within the road right-of-way.
- F. **Sidewalks.** 1) Outdoor pedestrian ramp compatibility with ADA requirements needs to be assessed. 2) Will building contain an elevator or interior stairs?
- G. **Landscaping/Lighting.** Landscaping/lighting evaluated as part of the Final Site Plan/PUD stage.
- G. **PUD Agreement.** 1) Develop PUD Agreement with performance guarantees for public benefits.
2) Recommend following conditions in PUD Agreement if Planning Commission supports the project:
 - a. A specific description of permitted building/site uses (i.e., uses that support recreational activities in the River Rouge Parkway park, including...).
 - b. Deed restrictions on future development of the site, prohibitions on expanded retail/commercial buildings/development of the property, and preservation of the open space as identified on the plans.
 - c. The historic restoration will be accomplished in compliance with the Secretary of Interior Standards for Rehabilitation of Historic Structures, and the work is overseen by an architectural historian that meets the minimum qualifications published in the Code of Federal Regulations (36 CFR Part 61).
 - d. The historic restoration will include restoration of the original steel sash window frames, and that the window frames will not be replaced with modern window frames. (Note: Replacement of the glass with modern glass is acceptable.)
 - e. The historic restoration will be done in compliance with any agreement with Wayne County, if historic restoration of the building is included in the purchase agreement for the property
- G. **Architectural Elevations.** 1) Provide floor plans.


CARLISLE/WORTMAN ASSOC., INC.
Sally M. Elmiger, AICP, LEED AP
Principal

#152-1901

cc: John Buzuvis
Marleta Barr



Memorandum

To: Greta Bolhuis, Planning & Community Development Director
Adam Gerlach, Assistant Director of Utilities

From: Shawn Keough, PE, Wade Trim

Date: June 6, 2024

Subject: Preliminary PUD Review for Donofrio Wilcox, LLC
230 Wilcox Road, City of Plymouth

We have reviewed the plans provided to Wade Trim on May 23, 2024, by the City of Plymouth. The plan set consists of 13 total sheets including detail sheets.

We offer the following comments for the Planning Commission consideration:

Public Sanitary Sewer

1. The plans show a proposed 6-inch sanitary sewer lead with a connection to an existing 36-inch sanitary sewer located along Edward Hines Drive.
2. We believe that the 36-inch sanitary sewer is under the jurisdiction of Wayne County.
3. The plans should be revised to show a 10-inch public sanitary sewer (for future ownership by the city) from the connection to the Wayne County sewer to the first proposed manhole.
4. The 6-inch sanitary lead should then be connected to the City's sanitary manhole.
5. A Part 41 permit from the State of Michigan Department of Environment, Great Lakes & Energy (EGLE) for new sanitary sewer construction will be required.

Public Water Supply

1. The plans do not currently show how the proposed project will be serviced by public water.
2. The City of Plymouth has existing 6-inch water main available at the intersection of Holbrook and Hardenberg. Future PUD plans should include an 8-inch water main extension along Wilcox from this intersection to the front of the proposed project.
3. The proposed location of the future water service into the renovated building should be shown on the plans.
4. We recommend at least one fire hydrant be installed at the end of the new 8-inch water main for fire protection. We also recommend that the City Fire Department provide input on the best location for the fire hydrant.
5. A permit from EGLE for new water system construction will be required for the new water main.

Storm Water Management Comments

1. The plans propose a storm water management system to manage the storm water runoff from the property. The plans provide significant design detail related to the two proposed bio swales. We have not reviewed the calculations that have been provided in full detail. We can complete that step as part of the final engineering review.
2. The two bio swales are proposed to outlet to an area on the property that is labeled on the landscape plan as “Middle River Rouge”.
3. Future submittals should provide additional detail regarding how the applicant plans to clean up the area described as “Middle River Rouge”, which we believe used to be the outlet of an old, abandoned race way when the historic mill was first in use.
4. The applicant is advised that they will be required to obtain a permit from the Wayne County Department of Public Services.
5. The applicant will be required to enter into the appropriate maintenance agreements with the City of Plymouth and Wayne County as part of their final approval of the project.

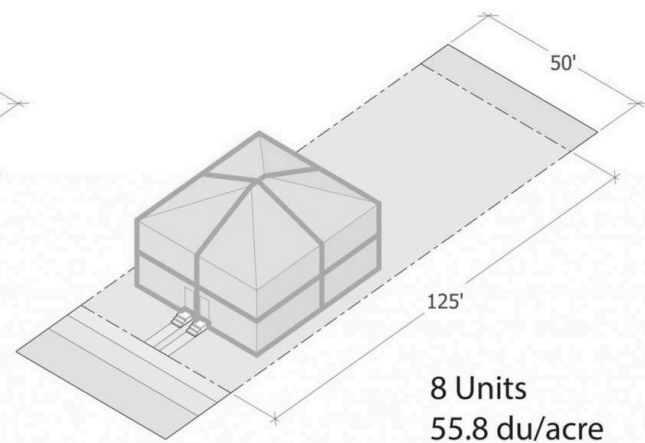
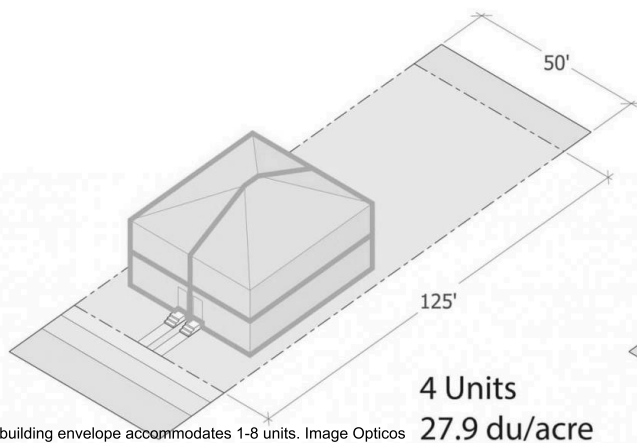
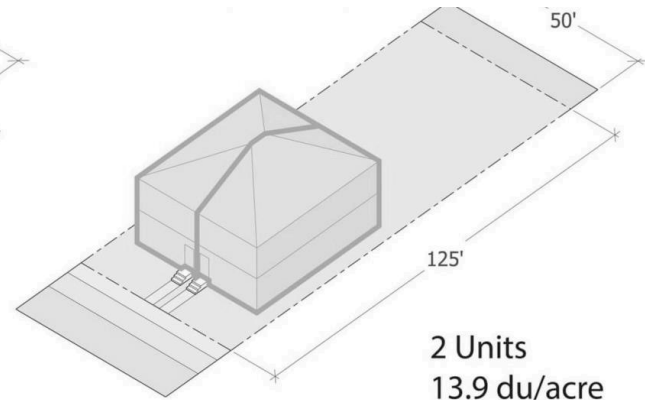
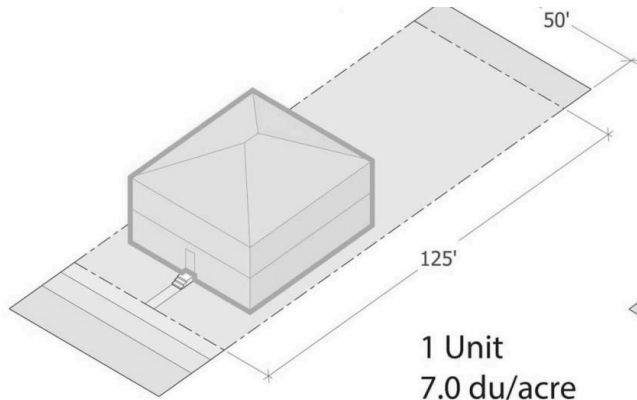
General Comments

1. The plans are signed by a professional engineer and a professional surveyor; however the plans also indicate in several places that the topographical survey was completed “by others”. If possible, it would be preferable to note the date of the survey and the entity that completed the survey.
2. The applicant has indicated that a Soil Erosion and Sedimentation Permit will be required. We agree.

We trust that these comments are helpful to the city and the Planning Commission as you review this development. Please feel free to contact us at any time with questions. We appreciate the opportunity to assist the City of Plymouth on this plan review.

Cc: John Buzuvis, Economic Development Manager
Chris Porman, Director of Municipal Services

PSQ (/publicsquare)



The same building envelope accommodates 1-8 units. Image Opticos


MISSING MIDDLE (/PUBLICSQUARE/CATEGORY/MISSING-MIDDLE)

Best practices for ending exclusive single-family zoning

Your city or state just allowed—or is considering allowing—multiple units on every lot. Here are six tips for successful implementation of the Missing Middle.

DAN PAROLEK (/node/5544) FEB. 25, 2020

 (<http://www.facebook.com/sharer/sharer.php?u=https%3A//www.cnu.org/node/7773&title=Best%20practices%20for%20ending%20exclusive%20single-family%20zoning>)

 (<http://twitter.com/intent/tweet?status=Best%20practices%20for%20ending%20exclusive%20single-family%20zoning%2Bhttps%3A//www.cnu.org/node/7773>)



([http://www.linkedin.com/shareArticle?](http://www.linkedin.com/shareArticle?mini=1&url=https%3A//www.cnu.org/node/7773&title=Best%20practices%20for%20ending%20exclusive%20single-family%20zoning&source=https%3A//www.cnu.org)

[mini=1&url=https%3A//www.cnu.org/node/7773&title=Best%20practices%20for%20ending%20exclusive%20single-family%20zoning&source=https%3A//www.cnu.org](https://www.cnu.org/node/7773&title=Best%20practices%20for%20ending%20exclusive%20single-family%20zoning&source=https%3A//www.cnu.org))



(mailto:?subject=Check%20out%20Best%20practices%20for%20ending%20exclusive%20single-family%20zoning&body=https%3A//www.cnu.org/publicsquare/2020/02/25/best-practices-ending-exclusive-single-family-zoning)

Last year was a groundbreaking one for housing policy and legislation to enable Missing Middle Housing (<https://missingmiddlehousing.com/>) across cities and states. Minneapolis adopted a policy to allow up to three units on any lot, even those zoned for single-family. Oregon passed HB 2001, effectively eliminating exclusive single-family zoning statewide and enabling a range of Missing Middle housing types, allowing up to two housing units on every residential lot within cities (and up to four housing units in cities with 25,000 or more residents). The current version of California's proposed SB50, although largely focused on development around major transit, would allow up to four units on single-family lots in geographic areas that meet certain criteria.

What these efforts illustrate is that using Missing Middle Housing can go a long way toward meeting housing needs in a variety of places. There are two important parts of this message:

- Incrementally increasing the allowed number of units/densities can substantially increase the number of housing units and address housing costs (see note below regarding using density/number of units carefully);
- Higher densities do not have to equate to larger buildings: House scale buildings/Missing Middle Housing can accommodate more units, more choices, *and* higher densities.

Here are six tips to effectively guide the evolution of single family neighborhoods into Missing Middle Housing neighborhoods integrating duplexes (<https://missingmiddlehousing.com/types/duplex-stacked>), triplexes, and fourplexes (<https://missingmiddlehousing.com/types/fourplex>). Many of these seem very basic, but are typically overlooked, and they do make a big difference in delivering high-quality results.

1. Regulate maximum building envelope/form & scale rather than number of units/density

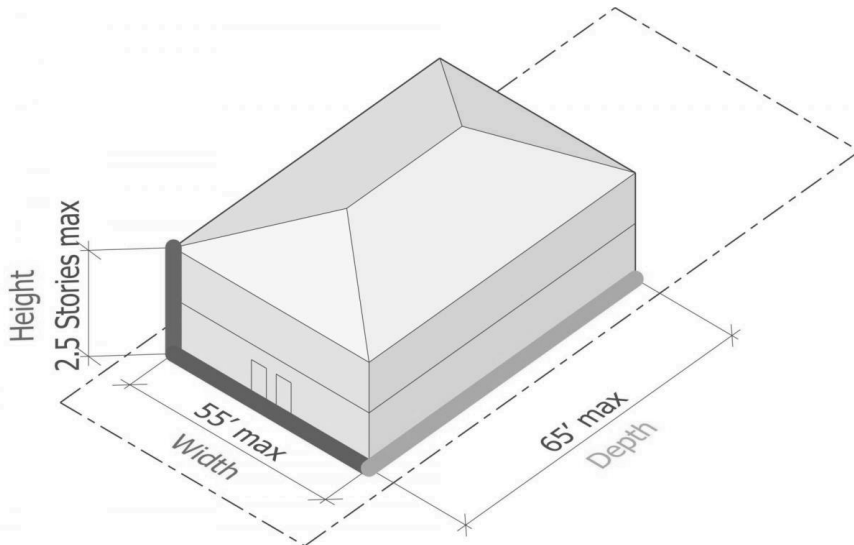
Most cities, policies, and bills focus on the number of units allowed on a lot. The good news about this approach is that after it is adopted it will increase the number of units compared to single family homes, but the bad news is that it will likely result in the delivery of the largest, most expensive/least affordable units that a builder can fit onto the site. How do you prevent this from happening and deliver more attainable options? Instead of defining a maximum number of units in policy, legislation, or

zoning, define a maximum width, depth, and height (i.e. an allowed building envelope) that can be publicly supported, and allow any number of units within that defined envelope or form (see image at top).

For example, if you regulate to allow a building that is a maximum of 50 feet wide, 55 feet deep, and two stories tall, you are allowing a 5,500 square foot building. If you define the maximum form and not the number of units, your policy and regulations would allow for a builder to deliver this as two 2,750 square foot units, four 1,375 square foot units, or eight 688 square foot units within that same allowed building envelope. Obviously, the 688 square foot units are going to be more cost-attainable than the larger units. If regulations/policy cap the number of units (as density has done in the past), it discourages smaller, more affordable units.

2. Carefully regulate building width and depth

This sounds very basic, but very few zoning codes directly regulate the size of a building envelope (form) with a maximum width and depth in addition to maximum height. This is important to deliver predictable built results. Setbacks and FAR only deliver predictable scale and form when lot sizes stay consistent, which is rarely the case in most neighborhoods. Height is typically regulated, but if a maximum width and maximum depth are not regulated and one lot is bigger than the one next to it, or if a builder has aggregated lots, the built result on the larger lot will be out of scale with its neighbors.



Maximum building width and maximum building depth need to be regulated in addition to height for predictable results. Image by Opticos Design.

Committing to this maximum scale of allowed “container/building envelope” is a great way to build support from community members for Missing Middle housing, because delivering more units does not have to mean larger buildings. Missing Middle types achieve more units, with higher densities (sorry, *I had to use the D word!*) within house scale buildings.

3. Be careful about allowed height

Ideally, Missing Middle Housing is no more than 2.5 stories in height to keep it “house scale.” Remember that Upper Missing Middle Housing, which is 3-plus stories, is still very relevant and necessary, but it should be considered a separate classification—and applied to different geographic locations—so as not to confuse the conversation or compromise support.

How is height measured in your zoning code? A maximum height measured at an eave will produce very different building forms than a maximum height measured from the highest point or even as an average height. If your heights are measured to the highest point, rather than to the top of the eave or parapet, your zoning may be encouraging/incentivizing flat-roofed buildings which likely will not be in character with existing structures.

4. Do not allow townhouses or single-family detached homes in which the ground floor is mostly parking

In most markets, if regulations allow, developers will deliver multiple “tall, skinny” single-family detached homes, or “tuck-under” townhouses (in which the ground floor is mostly parking with two-stories of living space above). You need to carefully consider and decide if the delivery of these housing types will meet policy intents, such as affordability goals, and the desired form and scale outcome. In some contexts it may, and in others it may not.

There are two issues with allowing these types:

- They do not deliver a good level of affordability, even compared to the single-family home they are replacing. A recent study by the Brookings Institute (<https://www.brookings.edu/research/gentle-density-can-save-our-neighborhoods/>) demonstrates that tearing down a single family home and replacing it with townhouses delivers more housing, but does not deliver more affordable prices in the same way that delivering a multiplex does. The study was done on a 4,500 square foot lot in the Washington D.C. metro, and compared prices of an existing single-family home purchased for \$1,000,000 to redevelopment of the site with either three townhouses or a six-plex condo building. While each of the townhouses would need to be sold for \$999,000 to support redevelopment, each of the six condos in the sixplex would need to sell for only \$579,000, a substantially lower and more attainable price point.
- The tall, skinny single-family homes and tuck-under townhouses do not typically deliver good urban form. Issues include ground floors occupied almost entirely by parking, low quality/minimal shared or private spaces, and excessive development of rear yards—causing privacy-related issues and eliminating the “green middle block” that even very urban neighborhoods in Brooklyn, San Francisco, and London have. Many downtown-adjacent neighborhoods in Houston are suffering from low-quality infill of these types that decreases the walkable urban quality of the neighborhoods.



5. Some single-family contexts are better than others for Missing Middle housing

Not all single-family contexts are as equally well-suited for Missing Middle Housing application. One of the main reasons that it made sense for Minneapolis to allow triplexes citywide is that a large majority of the city was laid out in a pattern of walkable streets and blocks with a mix of housing types prior to World War II. It does not hurt to enable Missing Middle Housing in more suburban/auto-dependent locations, but outside of creating complete new walkable neighborhoods in these contexts, it is less likely that builders will start building Missing Middle Housing or converting single family homes. This is because the market segments looking for Missing Middle Housing are not typically looking to live in auto-dependent places, and the economics are not likely in place to encourage this.

6. Communication and framing tips

Here are a few tips to frame the conversation for a broader range of housing choices in your community:

- **Avoid conversations about increasing density.** Instead, focus on increasing housing choice and attainability. I often recommend that we stop using the scary “D” word altogether because you will never convince any neighborhood that increasing density in and of itself is a good thing for their neighborhood. Consider terms such as Missing Middle Housing, house scale, housing diversity, and housing choices rather than density.

- **Document and photograph local examples and make this information easily available.** This seems simple, but very few cities have done this. Having a series of photo boards and/or an easily-accessible online collection of examples makes it easier for people without technical knowledge to relate to the planning, policy, and zoning decisions needed in order to permit these types. Simply photographing and documenting other physical characteristics of Missing Middle types (see example here on missingmiddlehousing.com (<http://missingmiddlehousing.com>)) is a great first step for any community. This is often the first step of a Missing Middle Scan (<https://missingmiddlehousing.com/services/>).
- **Personalize the conversation and tell stories:** Who has lived in one of these types? Who is currently living in these types? Whose kids or other relatives are living in one of these types? How are Missing Middle types beneficial for downsizing Baby Boomers? How are these types important for improving viability of neighborhood main streets and the coffee shops, restaurants, retail shops, etc. that are such a desired amenity in these neighborhoods? In order to take away some of the perceived stigma and help build support, form stories about who this housing will be for and who it has been for in the past.

There are many other barriers to Missing Middle Housing. One big barrier: onerous construction defect liability laws. These disincentivize and often prevent for-sale Missing Middle buildings from being built at all, because the inherent risk is not justifiable for small-scale builders. Another barrier: development fees that are charged per unit, regardless of whether the unit is 500 square feet or 5,000 square feet. This decreases the feasibility of small units.

Note: Dan Parolek's book, "Missing Middle Housing: Thinking Big and Building Small to Respond to the Housing Crisis," is now available for pre-order (<https://islandpress.org/books/missing-middle-housing>) from Island Press and will be available in late spring of 2020.

◀ MISSING MIDDLE (/PUBLICSQUARE/373) CODE REFORM (/PUBLICSQUARE/351)



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